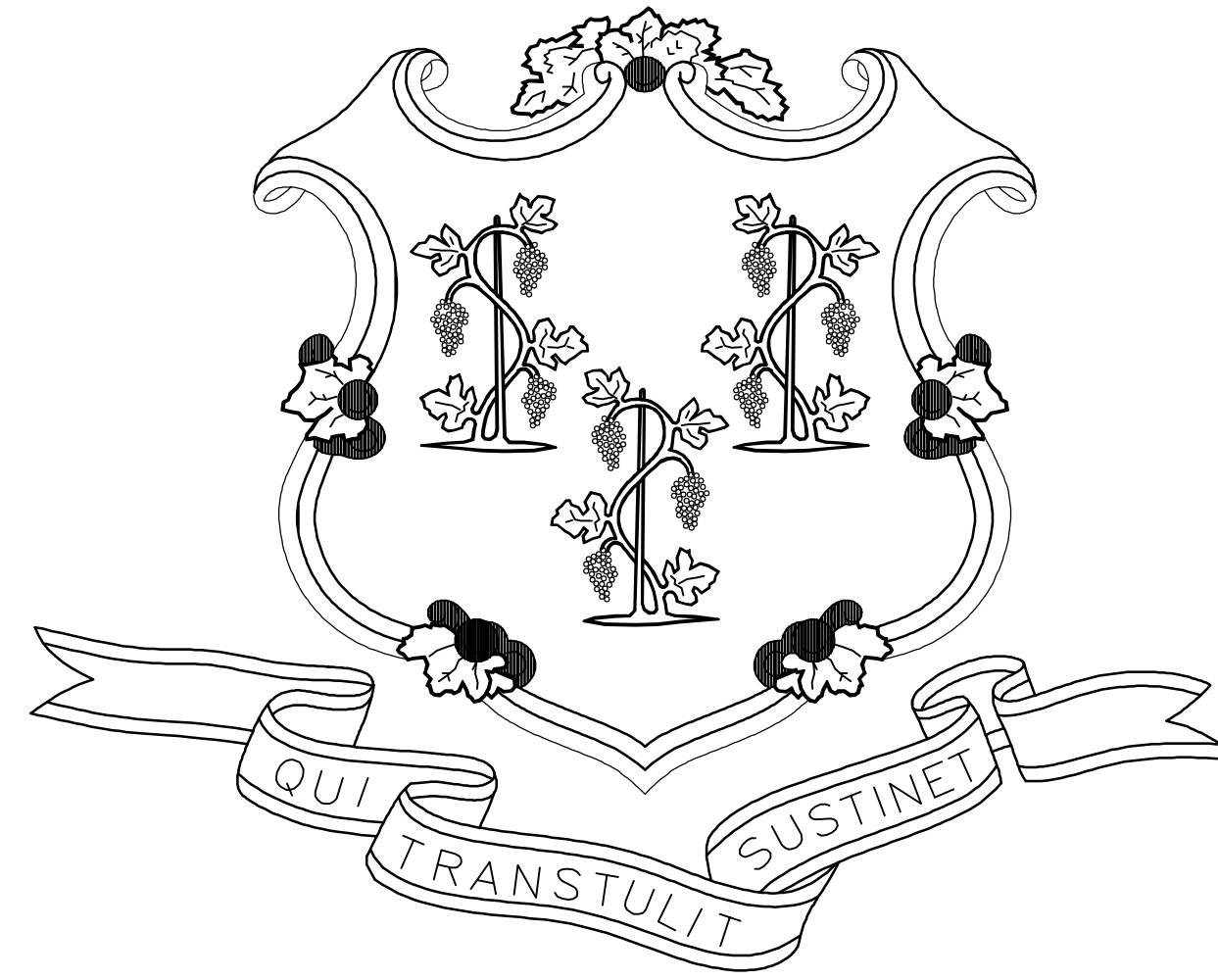


STATE OF CONNECTICUT



NED LAMONT, GOVERNOR

**THROUGH
CAPITAL REGION DEVELOPMENT AUTHORITY
100 COLUMBUS BOULEVARD, SUITE 500
HARTFORD, CT 06103**

**CHURCH STREET PARKING GARAGE
RENOVATIONS AND IMPROVEMENTS
HARTFORD, CT**

CRDA PROJECT NO. 21-016

STRUCTURAL ENGINEER:

DESMAN
Design Management

A DIVISION OF DESMAN, INC.
NEW YORK CHICAGO WASHINGTON, D.C. HARTFORD BOSTON CLEVELAND
175 Capital Boulevard, SUITE 402
Rocky Hill, CT 06067
P 860-563-1117 F 860-563-1118

CONTRACT DRAWINGS

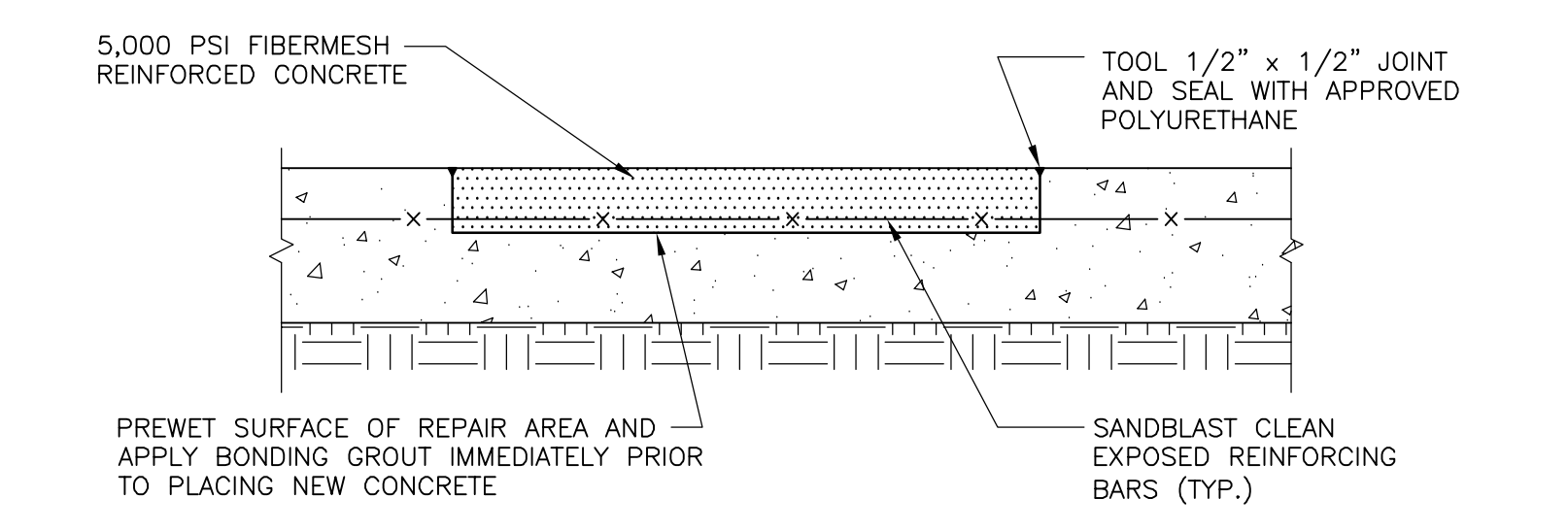
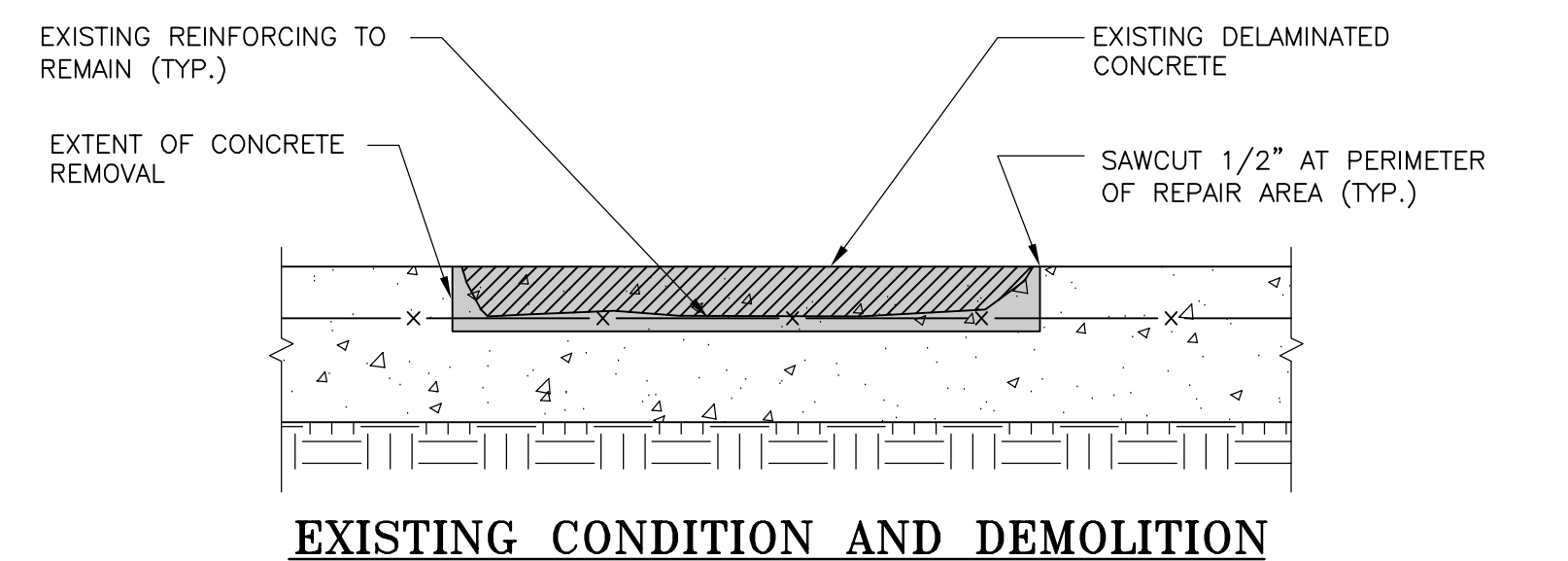
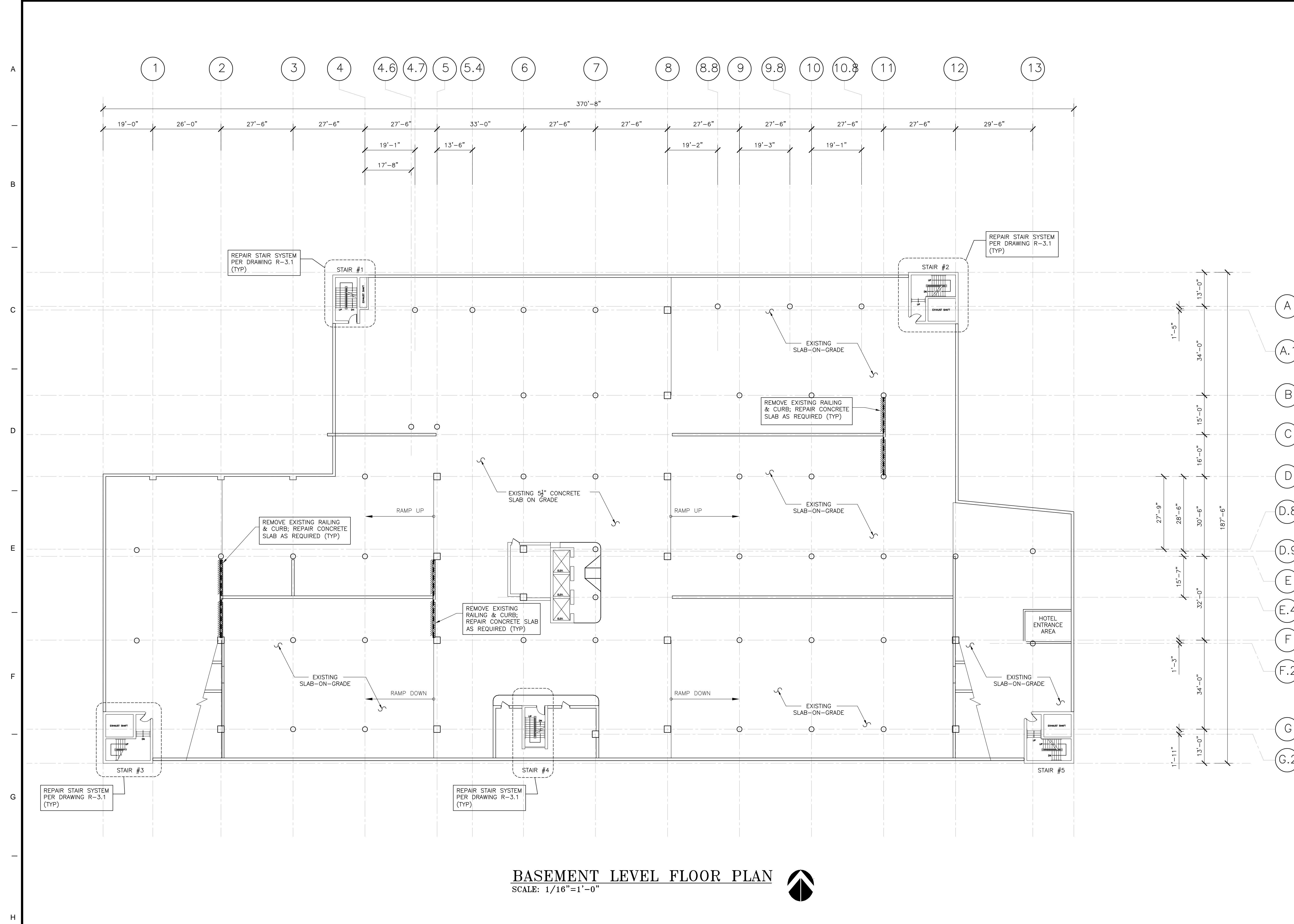
- R-1.0 BASEMENT LEVEL FLOOR PLAN
- R-1.1 FIRST LEVEL FLOOR PLAN
- R-1.2 SECOND LEVEL FLOOR PLAN
- R-1.3 THIRD LEVEL FLOOR PLAN
- R-1.4 FOURTH LEVEL FLOOR PLAN
- R-1.5 FIFTH LEVEL FLOOR PLAN
- R-1.6 SIXTH LEVEL FLOOR PLAN
- R-1.7 SEVENTH LEVEL FLOOR PLAN

- R-2.1 CONCRETE REPAIR DETAILS & SECTIONS
- R-2.2 CONCRETE REPAIR DETAILS & SECTIONS

- R-3.1 MISC. METAL DETAILS & SECTIONS
- R-3.2 MISC. METAL DETAILS & SECTIONS

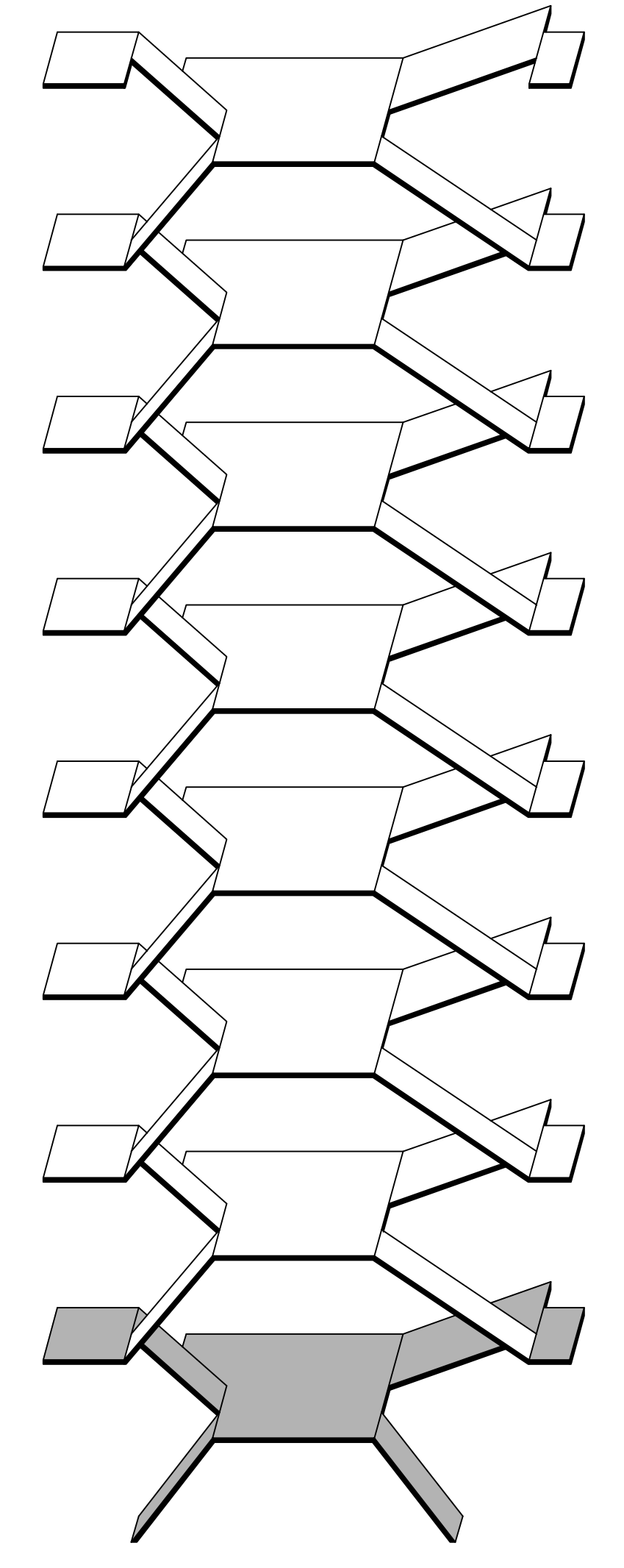
- R-4 WATERPROOFING DETAILS & SECTIONS

RELEASED FOR BID
APRIL 2021



- REPAIR PROCEDURE**
1. DETERMINE EXACT LOCATION OF DETERIORATED CONCRETE BY SOUNDING THE FLOOR SLAB IN THE PRESENCE OF THE ENGINEER.
 2. PROVIDE 1/2" DEEP SAWCUT AT PERIMETER OF REPAIR AREA.
 3. REMOVE CONCRETE TO SPECIFIED DEPTH AS SHOWN ON DETAIL AND AS DIRECTED BY THE ENGINEER IN THE FIELD USING CHIPPING HAMMERS.
 4. SANDBLAST CLEAN EXISTING REINFORCING.
 5. PREWET EXISTING CONCRETE AS REQUIRED AND PLACE NEW 5,000 PSI FIBERMESH CONCRETE.
 6. PROVIDE 1/2" x 1/2" TOOLED JOINT WITH SEALANT AROUND PERIPHERY OF REPAIR AREA.
 7. PROVIDE WET CURE AS PER SPECIFICATIONS.

A SLAB-ON-GRADE CONCRETE REPAIR
 R-1.0 SCALE: N.T.S.



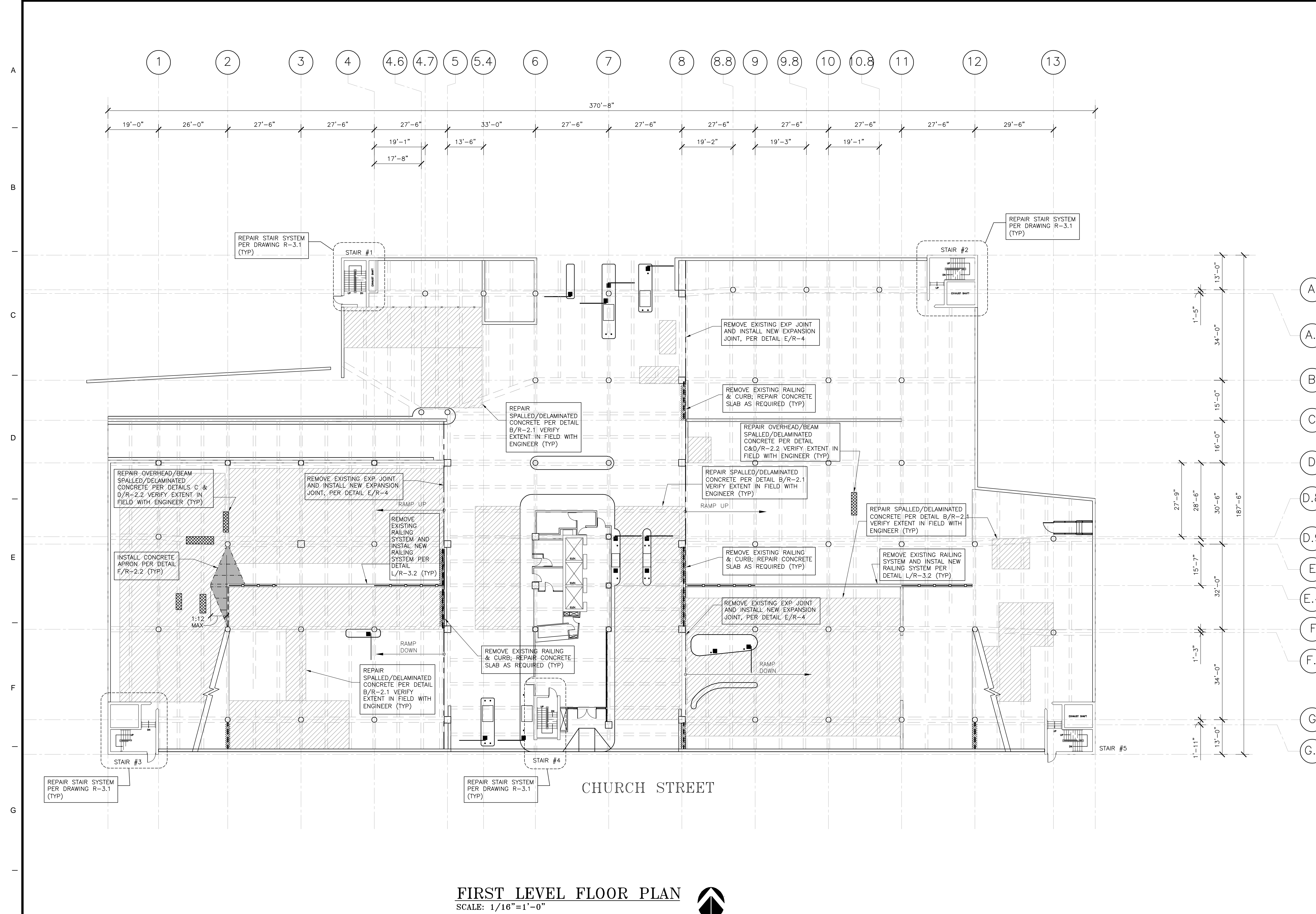
ISOMETRIC

BASEMENT LEVEL FLOOR PLAN
 SCALE: 1/16"=1'-0"

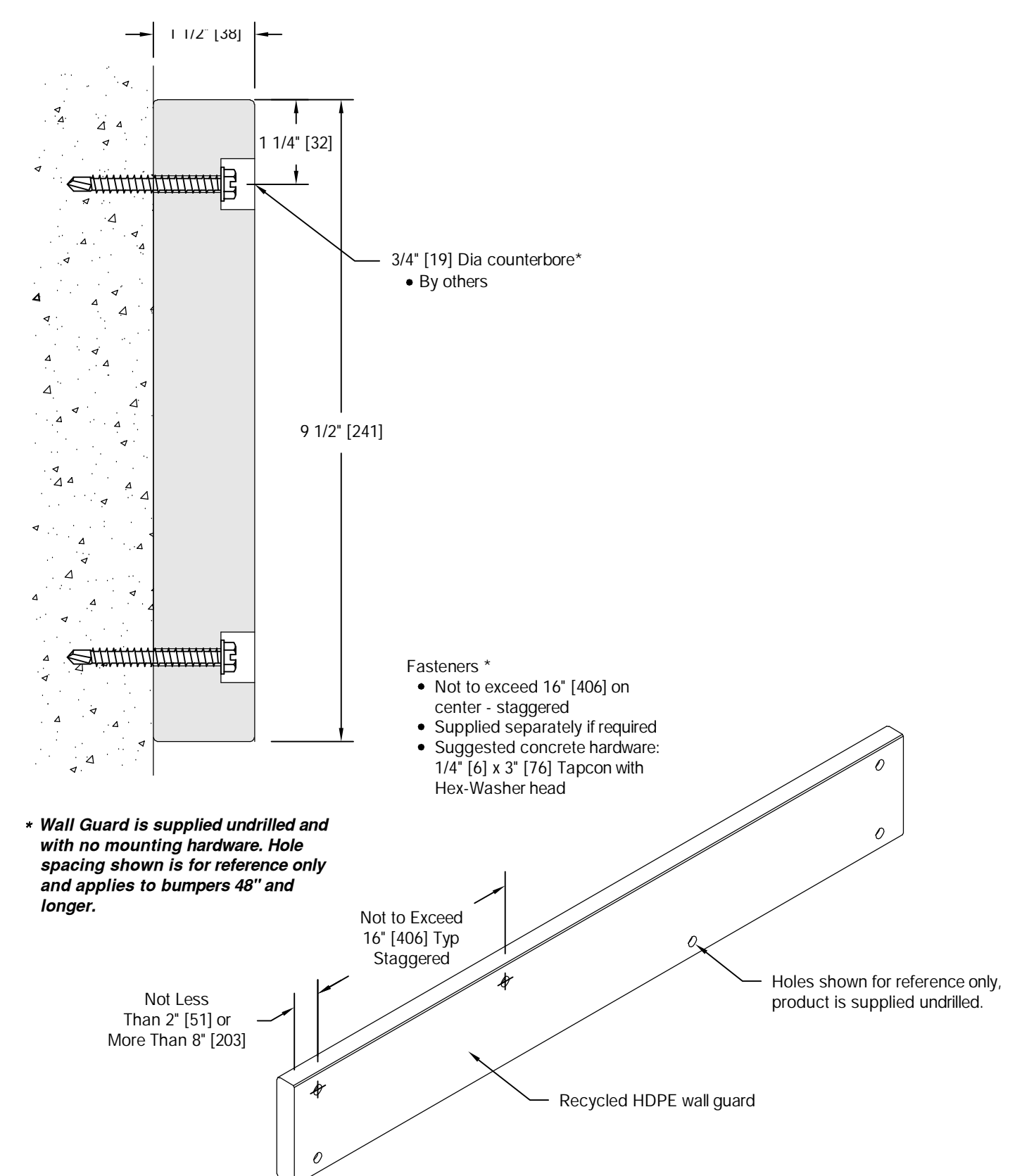
NOTES:

1. DO NOT SCALE OFF OF DRAWINGS.
2. LOCATIONS OF CRACKS, SCALING, SLAB REPAIR AREAS AND CONTROL/CONSTRUCTION JOINTS MAY VARY FROM THAT SHOWN ON THE PLAN. LOCATIONS OF CONCRETE REPAIRS SHALL BE DETERMINED BY CONTRACTOR, THEN REVIEWED AND APPROVED BY THE ENGINEER PRIOR TO STARTING DEMOLITION.
3. FOR CONCRETE DECK REPAIRS, SEE DRAWINGS R-2.1 & R-2.2. PRIOR TO START OF DEMOLITION AFFECTING A WORK AREA, CONTRACTOR SHALL LOCATE AND IDENTIFY ALL EXISTING, EMBEDDED POST-TENSION TENDONS (REGARDING THE UPPER LEVELS), BY APPROPRIATE METHODS (GROUND PENETRATING RADAR, OR APPROVED METHOD), AS APPROVED BY ENGINEER/OWNER. UPON LOCATING THE TENDONS, CONTRACTOR SHALL MARK THE LOCATIONS FOR VERIFICATION BY THE ENGINEER. DEMOLITION ACTIVITIES, AS REQUIRED FOR DESIGNATED CONCRETE REPAIRS, SHALL NOT BE STARTED UNTIL LOCATIONS OF TENDONS HAVE BEEN VERIFIED BY THE ENGINEER. IF DEMOLITION ACTIVITIES ARE STARTED BEFORE ENGINEER VERIFIES THE LOCATED TENDONS AS IDENTIFIED BY THE CONTRACTOR, CONTRACTOR SHALL PROCEED AT CONTRACTOR'S OWN RISK. SHOULD POST-TENSION TENDONS BE DAMAGED (OR ANY OTHER APPLICABLE UNFORESEEN CONDITIONS ARISE) WHILE CONTRACTOR IS PERFORMING AT CONTRACTOR'S OWN RISK, THEN CONTRACTOR SHALL REPAIR ALL DAMAGED TENDONS OR PERFORM OTHER APPLICABLE WORK (INCLUDING OF ANY COSTS ASSOCIATED WITH PROVISION OF SHORING, TRAFFIC CONTROL, ETC.) AT NO ADDITIONAL COST TO OWNER.
4. THE SUPPORTED CONCRETE SLAB SHALL BE SHORED AS REQUIRED PRIOR TO THE START OF CONCRETE DEMOLITION. SHORING MUST BE DESIGNED AND STAMPED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF CONNECTICUT. SHOP DRAWINGS OF THE SHORING DESIGN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO CONC. REMOVAL.
5. FOR WATERPROOFING REPAIRS, SEE DRAWING R-4. WORK SHALL INCLUDE THE INSTALLATION OF A HEAVY-DUTY, ELASTOMERIC TRAFFIC BEARING MEMBRANE, FULL-SYSTEM, MAINTAIN EXISTING CONCRETE SLOPE FOR PROPER DRAINAGE, MAKE LOCAL ADJUSTMENTS AS DEPICTED ON THE DRAWINGS OR AS DIRECTED BY THE ENGINEER, IN LIMITED LOCATIONS, WORK MAY INCLUDE THE PROVISION AND INSTALLATION OF LEVELING COURSE, AS RECOMMENDED BY THE MANUFACTURER AND APPROVED BY THE ENGINEER, TO MAINTAIN POSITIVE DRAINAGE. ALL WATERPROOFING MEMBRANE INSTALLATION WORK SHALL BE BASED UPON THE HORIZONTAL PROJECTION OF THE WORK. ALL VERTICAL SURGE APPLICATIONS (I.E. CURBING, ETC.) SHALL BE INCIDENTAL TO THE WORK. CONTRACTOR SHALL NOTE THAT ALL CRACK REPAIR, CONTROL/CONSTRUCTION JOINT REPAIR, AS WELL AS COVE JOINT REPAIR, SHALL BE INCIDENTAL TO THE APPLICATION OF THE TRAFFIC BEARING MEMBRANE, AND SHALL BE INCLUDED IN THE CONTRACT PRICE.
6. WORK SHALL INCLUDE ADDRESSING ANY SHALLOW-DEPTH DEVIATIONS IN THE CONCRETE SURFACE. THESE SHALLOW-DEPTH REPAIR LOCATIONS, INCLUDING REMOVAL OF OBSTRUCTIONS TO THE LEVEL SURFACE (SUCH AS ABANDONED PIECES OF CONDUIT, FORMER ANCHORS, ETC.) SHALL TYPICALLY BE CONSIDERED ADDRESSABLE BY INSTALLATION OF A LEVELING COURSE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS; ALL LOCATIONS SHALL BE REVIEWED BY THE MANUFACTURER AND APPROVED BY THE ENGINEER PRIOR TO START OF WORK (TO CONFIRM ABANDONMENT AS APPLICABLE).
7. CONTRACTOR SHALL DE-ENERGIZE AND/OR REMOVE ALL ABANDONED EMBEDDED CONDUIT IN SLAB SYSTEM AND/OR REROUTE ELECTRICAL CONDUIT AS REQUIRED TO FACILITATE CONCRETE DECK REPAIRS. THE INTENT BEING THAT EXISTING ACTIVE CONDUIT SHALL REMAIN, THEREFORE THE CONTRACTOR COORDINATE ALL STRUCTURAL, ELECTRICAL AND MECHANICAL ASPECTS OF WORK, LOCATING ALL EMBEDDED MATERIALS PRIOR TO START OF WORK. IF DEMOLITION ACTIVITIES ARE STARTED BEFORE ENGINEER VERIFIES THE LOCATED CONDUITS (OR OTHER EMBEDDED MATERIALS) AS IDENTIFIED BY THE CONTRACTOR, CONTRACTOR SHALL PROCEED AT CONTRACTOR'S OWN RISK. SHOULD CONDUITS (OR OTHER EMBEDDED MATERIALS) BE DAMAGED (OR ANY OTHER APPLICABLE UNFORESEEN CONDITIONS ARISE) WHILE CONTRACTOR IS PERFORMING AT CONTRACTOR'S OWN RISK, THEN CONTRACTOR SHALL REPAIR ALL DAMAGED CONDUITS (OR OTHER EMBEDDED MATERIALS) OR PERFORM OTHER APPLICABLE WORK (INCLUDING OF ANY COSTS ASSOCIATED WITH PROVISION OF SHORING, TRAFFIC CONTROL, ETC.) AT NO ADDITIONAL COST TO OWNER.
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9. TEMPORARY LIGHTING AND/OR ELECTRICAL SERVICE TO EITHER THE WORK AREA OR OTHER AREAS IN THE FACILITY IMPACTED BY THE REPAIRS SHALL BE THE CONTRACTOR'S RESPONSIBILITY. ALL AREAS UTILIZED BY GARAGE PATRONS SHALL BE ADEQUATELY LIGHTED (MINIMUM 10 FOOT CANDLES AT CENTERLINE OF DRIVE LANES) DURING ALL PHASES OF THE WORK.
10. CONTRACTOR SHALL INSTALL FULL HEIGHT PARTITIONS AROUND AND BELOW ALL WORK AREAS WHERE DEMOLITION AND SUBSEQUENT CONCRETE REPAIRS ARE BEING PERFORMED. PARTITIONS SHALL BE CONSTRUCTED TO PREVENT BOTH DUST AND FALLING DEBRIS FROM LEAVING THE CONFINES OF THE WORK AREA AS WELL AS TO PROVIDE NOISE ABATEMENT. PARTITIONS SHALL BE CONFIGURED TO ATTENUATE NOISE IN THE GARAGE, THROUGH THE USE OF QUILTED INSULATION BLANKETS, AND TO PREVENT UNAUTHORIZED ACCESS INTO ACTIVE WORK AREAS. PARTITION SYSTEMS SHALL BE CONSTRUCTED OF 2 X 4 STUDS AND A MINIMUM OF 3/8" PLYWOOD PANELS, 48" HIGH WITH PLASTIC SHEETING INSTALLED TO THE DECK ABOVE. THE PARTITION SYSTEMS SHALL BE FIRE-RATED AND ALL EXPOSED (EXTERIOR) PLYWOOD SURFACES SHALL BE PAINTED WHITE.
11. THE CONTRACTOR SHALL PROVIDE MAXIMUM VEHICULAR AND PEDESTRIAN MOBILITY THROUGH AND/OR IN THE VICINITY OF THE PROJECT AREA. PROPER COORDINATION BETWEEN THE CONTRACTOR AND GARAGE MANAGEMENT STAFF SHALL BE REQUIRED IN THE SEQUENCING AND SCHEDULING OF REPAIRS. MOVEMENT OF MATERIAL, CONSTRUCTION DEBRIS AND THE REMOBILIZATION OF NEW WORK AREAS SHALL NOT TAKE PLACE DURING PERIODS OF PEAK TRAFFIC FLOW. BOTH VEHICULAR AND PEDESTRIAN TRAFFIC SHALL BE MAINTAINED AT ALL TIMES, AND CONTRACTOR SHALL INSTALL TRAFFIC CONTROL SIGNS AND LIGHTS FOR THE SAFE PASSAGE THROUGH THE GARAGE, THROUGH THE STAIRS, AND AT THE ENTRANCES AND EXITS DURING THE ENTIRE CONSTRUCTION PERIOD. ONLY ONE STAIR AND/OR ONE ELEVATOR MAY BE TAKEN OUT OF SERVICE AT ANY GIVEN TIME BUT NEVER BOTH AT THE SAME STAIR/ELEVATOR TOWER, PROPERLY COORDINATED WITH OWNER IN ADVANCE. SIGNAGE MAY BE REQUIRED ON THE SURROUNDING STREETS IF REPAIR WORK IS TO BE ACCOMPLISHED ON THE ENTRY/EXIT RAMPS. CONTRACTOR IS TO OBTAIN PERMITS FROM APPROPRIATE AGENCIES.
12. FOLLOWING COMPLETION OF MEMBRANE WORK, CONTRACTOR SHALL APPLY NEW MARKINGS FOR ALL PARKING STALLS, DIRECTIONAL ARROWS, CROSSWALKS, CURBS, CENTERLINES, TEXT AND OTHER TRAFFIC MARKINGS LOCATED THROUGHOUT THE GARAGE. APPLICATION OF STRIPING SHALL TYPICALLY BE PERFORMED TO MATCH PRE-EXISTING CONDITIONS OR AS OTHERWISE DIRECTED BY THE ENGINEER. HOWEVER ALL HANDICAP PARKING STALLS SHALL BE UPDATED ACCORDINGLY PER CURRENT REQUIREMENTS AND QUANTITY/LOCATION OF HANDICAP PARKING STALLS SHALL BE VERIFIED PRIOR TO START OF WORK. ALL CURBINGS SHALL BE PAINTED, BOTH THOSE CURRENTLY PAINTED AND THOSE NOT. ANY AND ALL CONFLICTING MARKINGS SHALL BE REMOVED IN THEIR ENTIRETY BY APPROVED METHOD. OWNER/ENGINEER SHALL BE REQUIRED TO REVIEW STRIPING LAYOUT PRIOR TO THIS WORK BEING PERFORMED.

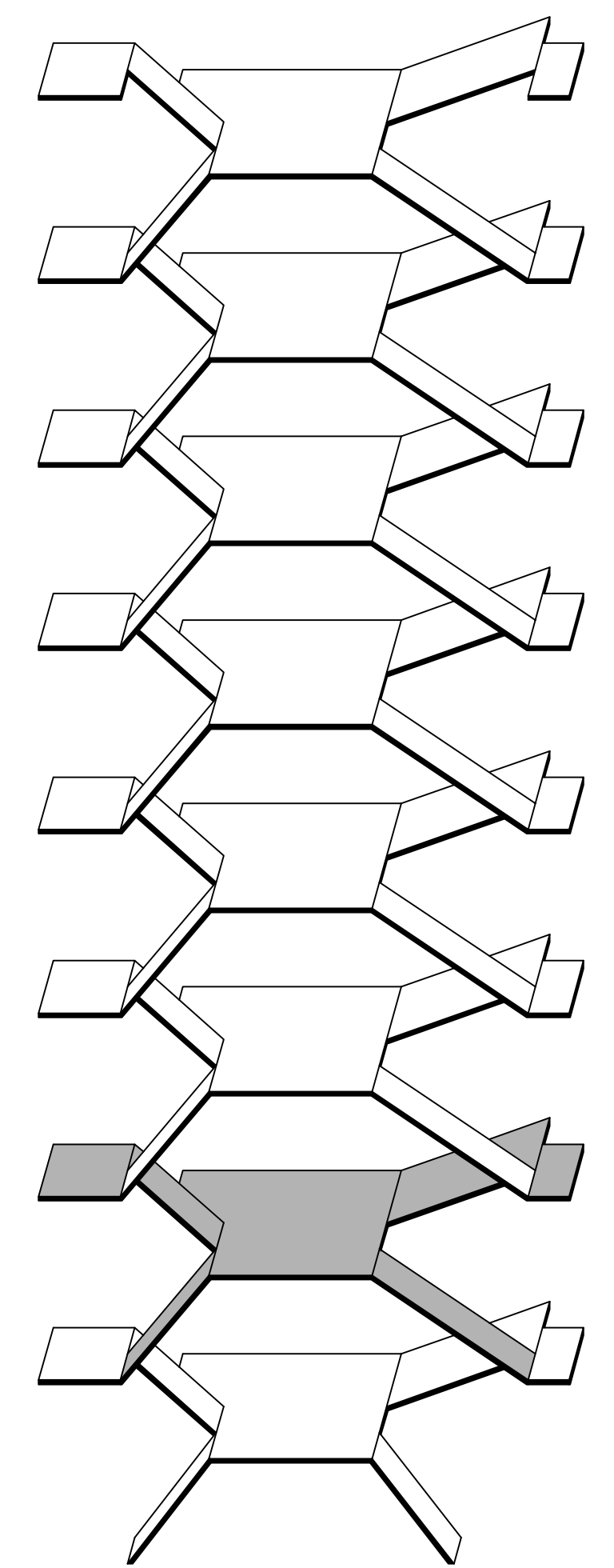
Drawing Title BASEMENT LEVEL FLOOR PLAN		STATE OF CONNECTICUT CAPITAL REGION DEVELOPMENT AUTHORITY	
Revisions		Project Prepared By: DESMAN Design Management	Date: APRIL 2021
No.	Date	Description	Scale: AS NOTED
		Project: CHURCH STREET PARKING GARAGE RENOVATIONS & IMPROVEMENTS	Production Leader: -
		Location: HARTFORD, CONNECTICUT	Project Manager: -
			Peer Reviewer: -
			Drawing Number: R-1.0
CAD Number:		Project Number: 21-016	



FIRST LEVEL FLOOR PLAN
SCALE: 1/16"=1'-0"



INSTALLATION OF VEHICULAR BUMPER GUARD
R-1.1 SCALE: N.T.S.

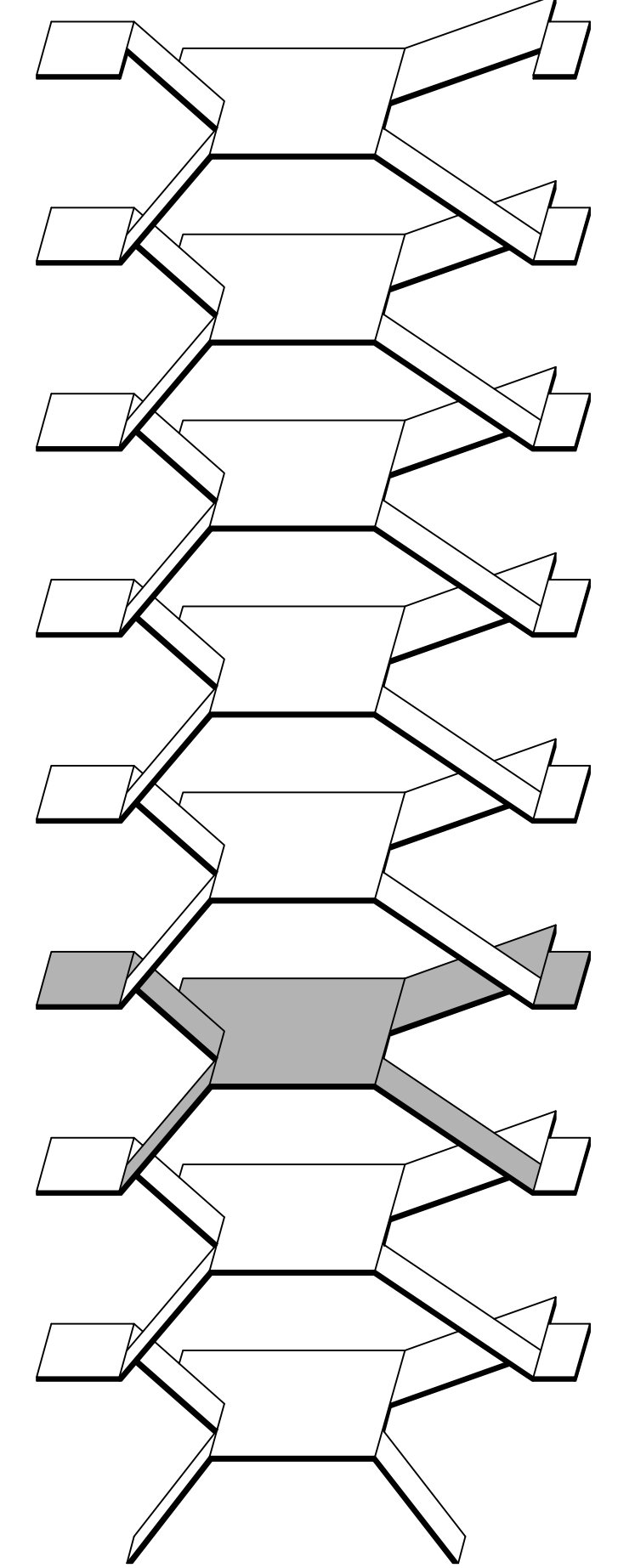
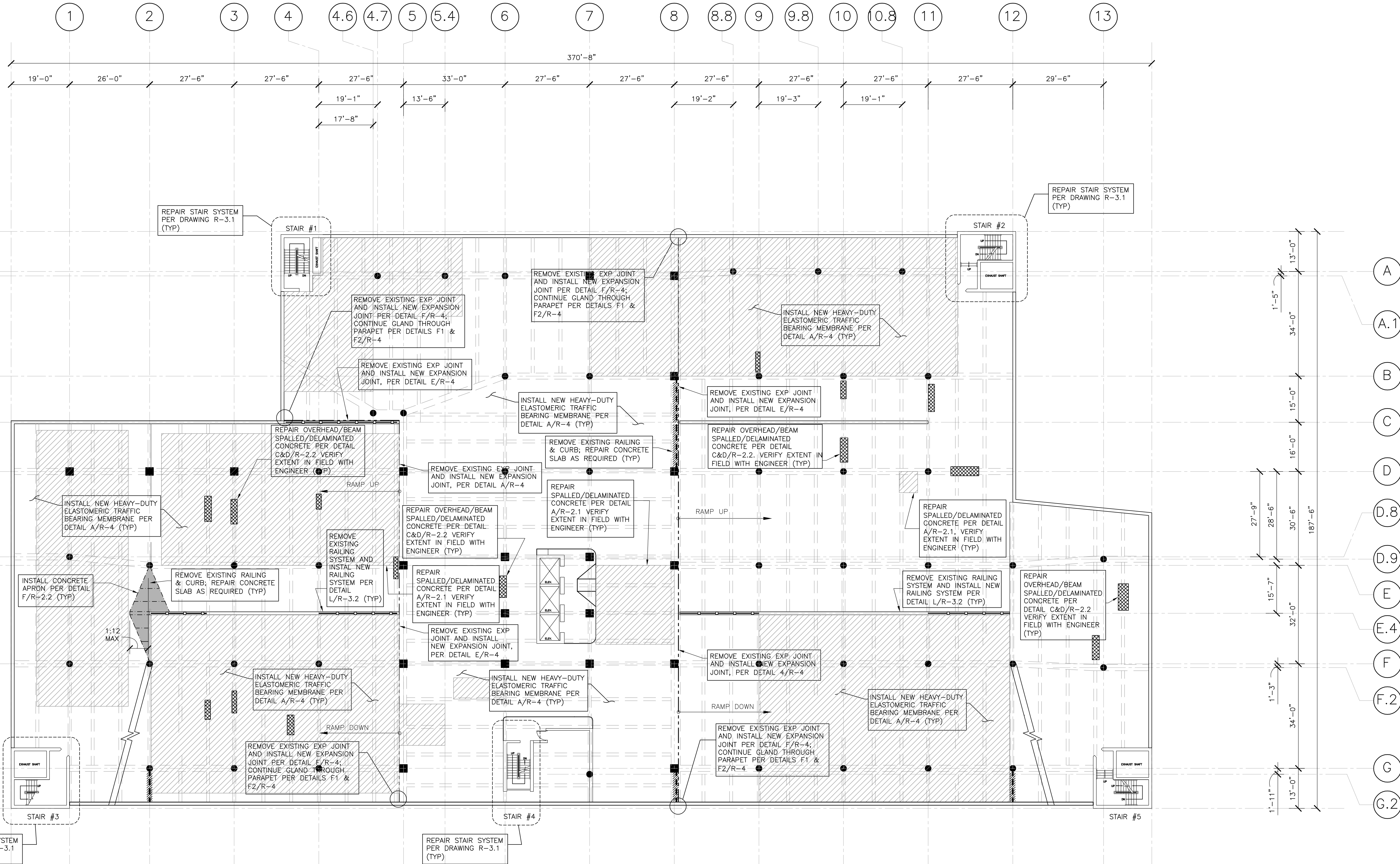


ISOMETRIC

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Drawing Title		STATE OF CONNECTICUT CAPITAL REGION DEVELOPMENT AUTHORITY	
FIRST LEVEL FLOOR PLAN		DESIGN MANAGEMENT	
Revisions		Prepared By:	Date: APRIL 2021
No.	Date	Description	AS NOTED
		Project:	CHURCH STREET PARKING GARAGE RENOVATIONS & IMPROVEMENTS
		Project Manager:	
		Production Leader:	
		Project Engineer:	
		Permit Reviewer:	
		Drawing Number:	R-1.1
CAD Number:		Project Number:	21-016

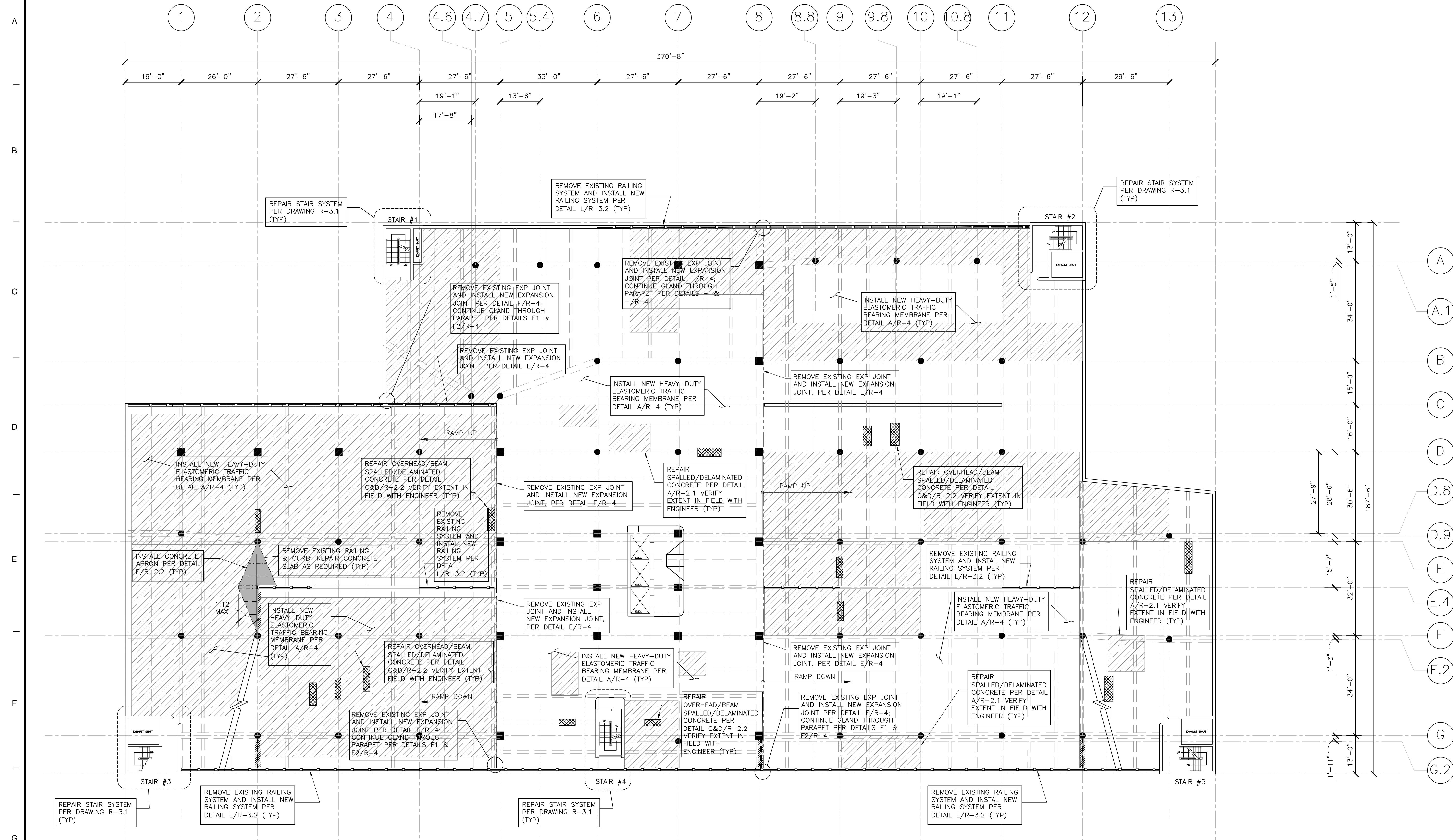


SECOND LEVEL FLOOR PLAN
SCALE: 1/16"=1'-0"

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- CONTRACTOR SHALL DE-ENERGIZE AND/OR REMOVE ALL ABANDONED EMBEDDED CONDUIT IN SLAB SYSTEM AND/OR REROUTE ELECTRICAL CONDUIT AS REQUIRED TO FACILITATE CONCRETE DECK REPAIRS. THE INTENT BEING THAT EXISTING ACTIVE CONDUIT SHALL REMAIN, THEREFORE THE CONTRACTOR COORDINATE ALL STRUCTURAL, ELECTRICAL AND MECHANICAL ASPECTS OF WORK. LOCATING ALL EMBEDDED MATERIALS PRIOR TO START OF WORK. IF DEMOLITION ACTIVITIES ARE STARTED BEFORE ENGINEER VERIFIES THE LOCATED CONDUITS (OR OTHER EMBEDDED MATERIALS) AS IDENTIFIED BY THE CONTRACTOR, CONTRACTOR SHALL PROCEED AT CONTRACTOR'S OWN RISK. SHOULD CONDUITS (OR OTHER EMBEDDED MATERIALS) BE DAMAGED (OR ANY OTHER APPLICABLE UNFORESSEEN CONDITIONS ARISE) WHILE CONTRACTOR IS PERFORMING AT CONTRACTOR'S OWN RISK, THEN CONTRACTOR SHALL REPAIR ALL DAMAGED CONDUITS (OR OTHER EMBEDDED MATERIALS) OR PERFORM OTHER APPLICABLE WORK (INCLUSIVE OF ANY COSTS ASSOCIATED WITH PROVISION OF SHORING, TRAFFIC CONTROL, ETC.) AT NO ADDITIONAL COST TO OWNER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REMOVAL, STORAGE & RE-INSTALLATION OR REPLACEMENT OF ALL EXISTING SLAB ATTACHMENTS BELOW AND/OR ADJACENT TO THE WORK AREA (I.E. PLUMBING, ELECTRICAL CONDUIT, ELECTRICAL FIXTURES, SIGNAGE, ETC.) IF NOT REMOVED, SLAB ATTACHMENTS SHALL BE PROTECTED FROM DAMAGE AND TEMPORARILY SUPPORTED AS REQUIRED.
- TEMPORARY LIGHTING AND/OR ELECTRICAL SERVICE TO EITHER THE WORK AREA OR OTHER AREAS IN THE FACILITY IMPACTED BY THE REPAIRS SHALL BE THE CONTRACTOR'S RESPONSIBILITY. ALL AREAS UTILIZED BY GARAGE PATRONS SHALL BE ADEQUATELY LIGHTED (MINIMUM 10 FOOT CANDLES AT CENTERLINE OF DRIVE LANES) DURING ALL PHASES OF THE WORK.
- CONTRACTOR SHALL INSTALL FULL HEIGHT PARTITIONS AROUND AND BELOW ALL WORK AREAS WHERE DEMOLITION AND SUBSEQUENT CONCRETE REPAIRS ARE BEING PERFORMED. PARTITIONS SHALL BE CONSTRUCTED TO PREVENT BOTH DUST AND FALLING DEBRIS FROM LEAVING THE CONFINES OF THE WORK AREA AS WELL AS TO PROVIDE NOISE ABATEMENT. PARTITIONS SHALL BE CONFIGURED TO ATTENUATE NOISE IN THE GARAGE, THROUGH THE USE OF QUIETED INSULATION BLANKETS, AND TO PREVENT UNAUTHORIZED ACCESS INTO ACTIVE WORK AREAS. PARTITION SYSTEMS SHALL BE CONSTRUCTED OF 2 X 4 STUDS AND A MINIMUM OF 3/8" PLYWOOD PANELS. 48" HIGH WITH PLASTIC SHEETING INSTALLED TO THE DECK ABOVE. THE PARTITION SYSTEMS SHALL BE FIRE-RATED AND ALL EXPOSED (EXTERIOR) PLYWOOD SURFACES SHALL BE PAINTED WHITE.
- THE CONTRACTOR SHALL PROVIDE MAXIMUM VEHICULAR AND PEDESTRIAN MOBILITY THROUGH AND/OR IN THE VICINITY OF THE PROJECT AREA. PROPER COORDINATION BETWEEN THE CONTRACTOR AND GARAGE MANAGEMENT STAFF SHALL BE REQUIRED IN THE SEQUENCING AND SCHEDULING OF REPAIRS. MOVEMENT OF MATERIAL, CONSTRUCTION DEBRIS AND THE REMOVAL OF NEW WORK AREAS SHALL NOT TAKE PLACE DURING PERIODS OF PEAK TRAFFIC FLOW. BOTH VEHICULAR AND PEDESTRIAN TRAFFIC SHALL BE MAINTAINED AT ALL TIMES, AND CONTRACTOR SHALL INSTALL TRAFFIC CONTROL SIGNS AND LIGHTS FOR THE SAFE PASSAGE THROUGH THE GARAGE, THROUGH THE STAIRS, AND AT THE ENTRANCES AND EXITS DURING THE ENTIRE CONSTRUCTION PERIOD. ONLY ONE STAIR AND/OR ONE ELEVATOR MAY BE TAKEN OUT OF SERVICE AT ANY GIVEN TIME BUT NEVER BOTH AT THE SAME STAIR/ELEVATOR TOWER, PROPERLY COORDINATED WITH OWNER IN ADVANCE. SIGNAGE MAY BE REQUIRED ON THE SURROUNDING STREETS IF REPAIR WORK IS TO BE ACCOMPLISHED ON THE ENTRY/EXIT RAMP. CONTRACTOR IS TO OBTAIN PERMITS FROM APPROPRIATE AGENCIES.
- FOLLOWING COMPLETION OF MEMBRANE WORK, CONTRACTOR SHALL APPLY NEW MARKINGS FOR ALL PARKING STALLS, DIRECTIONAL ARROWS, CROSSWALKS, CURBS, CENTERLINES, TEXT AND OTHER TRAFFIC MARKINGS LOCATED THROUGHOUT THE GARAGE. APPLICATION OF STRIPING SHALL TYPICALLY BE PERFORMED TO MATCH PRE-EXISTING CONDITIONS OR AS OTHERWISE DIRECTED BY THE ENGINEER. HOWEVER ALL HANDICAP PARKING STALLS SHALL BE UPDATED ACCORDINGLY PER CURRENT REQUIREMENTS AND QUANTITY/LOCATION OF HANDICAP PARKING STALLS SHALL BE VERIFIED PRIOR TO START OF WORK. ALL CURBINGS SHALL BE PAINTED, BOTH THOSE CURRENTLY PAINTED AND THOSE NOT. ANY AND ALL CONFLICTING MARKINGS SHALL BE REMOVED IN THEIR ENTIRETY BY APPROVED METHOD. OWNER/ENGINEER SHALL BE REQUIRED TO REVIEW STRIPING LAYOUT PRIOR TO THIS WORK BEING PERFORMED.

Drawing Title SECOND LEVEL FLOOR PLAN		STATE OF CONNECTICUT CAPITAL REGION DEVELOPMENT AUTHORITY	
Revisions		Date: APRIL 2021	
No.	Date	Description	Drawn By: AS NOTED
			Project Manager: -
			Project Architect: -
			Peer Reviewer: -
			Drawing Number: R-1.2
CAD Number: -		Project Number: 21-016	

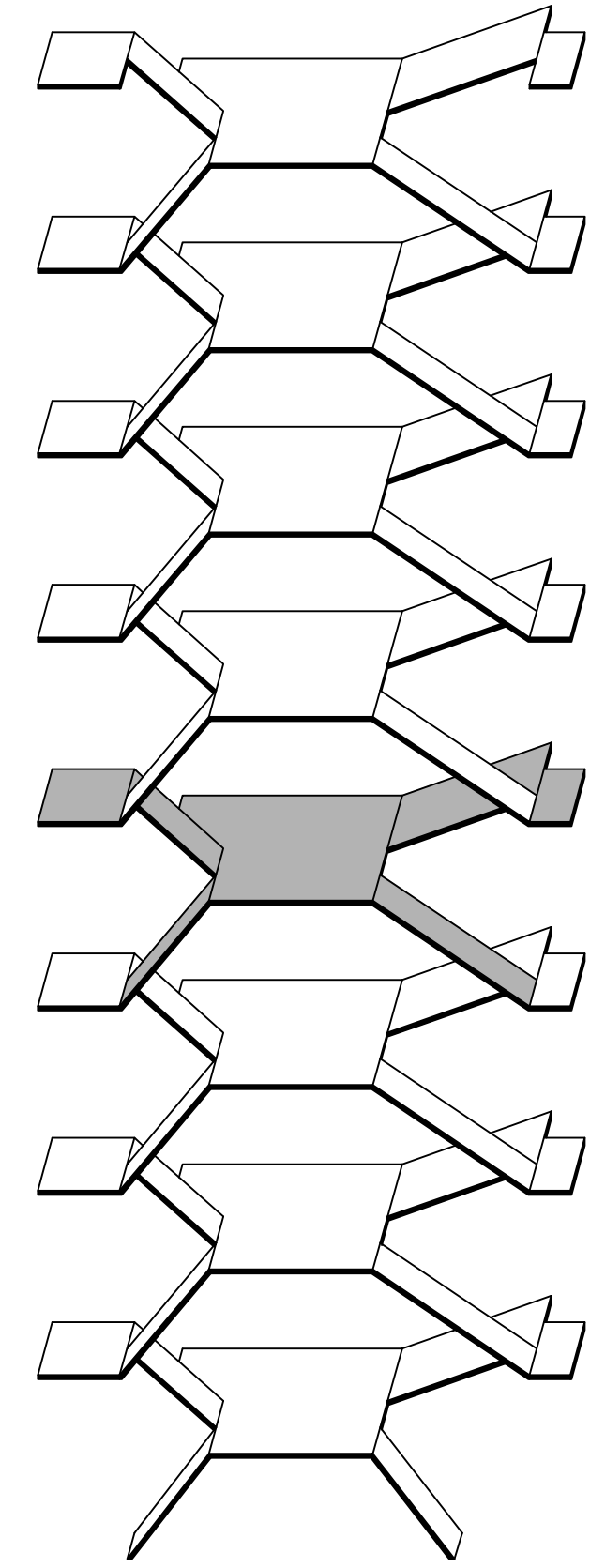


THIRD LEVEL FLOOR PLAN
 SCALE: 1/16"=1'-0"



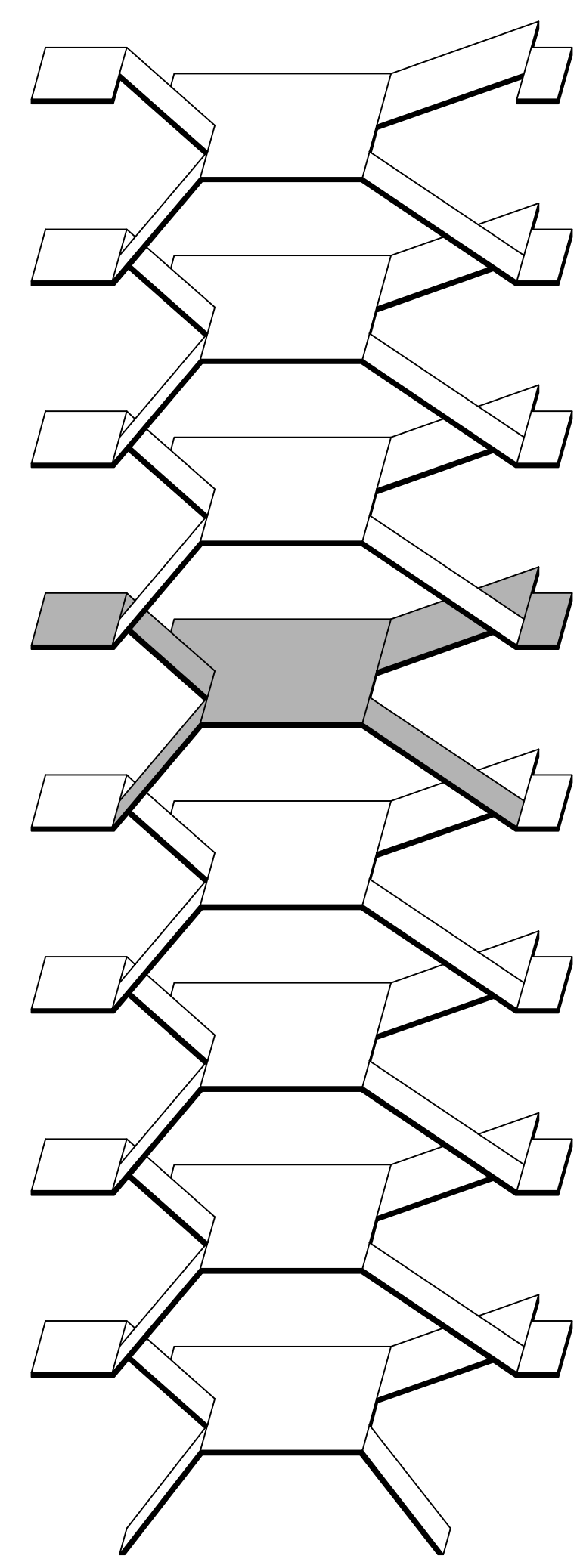
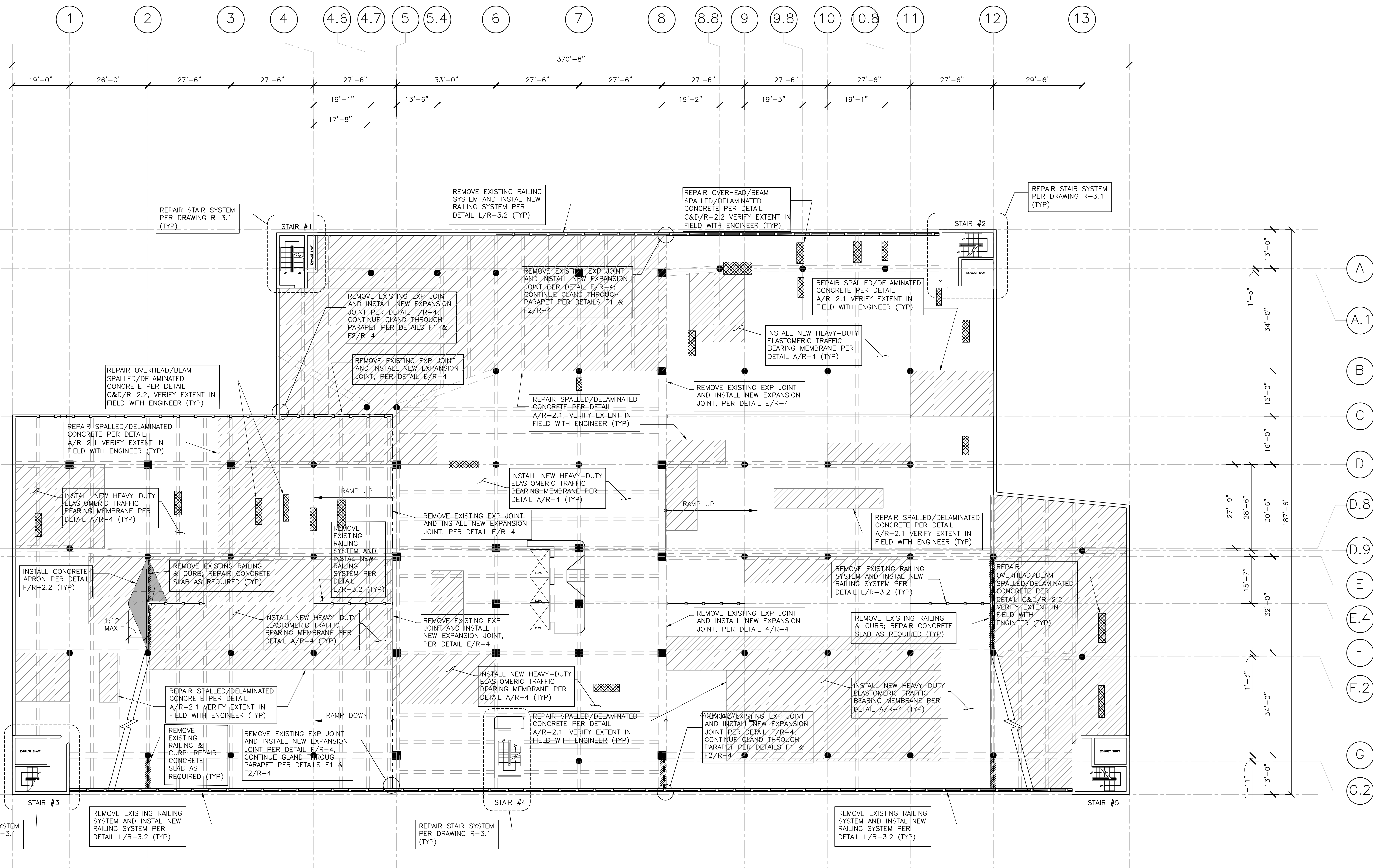
- NOTES:**
- DO NOT SCALE OFF OF DRAWINGS.
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 - FOR CONCRETE DECK REPAIRS, SEE DRAWINGS R-2.1 & R-2.2. PRIOR TO START OF DEMOLITION AFFECTING A WORK AREA, CONTRACTOR SHALL LOCATE AND IDENTIFY ALL EXISTING, EMBEDDED POST-TENSION TENDONS (REGARDING THE UPPER LEVELS), BY APPROPRIATE METHODS (GROUND PENETRATING RADAR, OR APPROVED METHOD), AS APPROVED BY ENGINEER/OWNER. UPON LOCATING THE TENDONS, CONTRACTOR SHALL MARK THE LOCATIONS FOR VERIFICATION BY THE ENGINEER. DEMOLITION ACTIVITIES, AS REQUIRED FOR DESIGNATED CONCRETE REPAIRS, SHALL NOT BE STARTED UNTIL LOCATIONS OF TENDONS HAVE BEEN VERIFIED BY THE ENGINEER. IF DEMOLITION ACTIVITIES ARE STARTED BEFORE ENGINEER VERIFIES THE LOCATED TENDONS AS IDENTIFIED BY THE CONTRACTOR, CONTRACTOR SHALL PROCEED AT CONTRACTOR'S OWN RISK. SHOULD POST-TENSION TENDONS BE DAMAGED (OR ANY OTHER APPLICABLE UNFORESEEN CONDITIONS ARISE) WHILE CONTRACTOR IS PERFORMING AT CONTRACTOR'S OWN RISK, THEN CONTRACTOR SHALL REPAIR ALL DAMAGED TENDONS OR PERFORM OTHER APPLICABLE WORK (INCLUSIVE OF ANY COSTS ASSOCIATED WITH PROVISION OF SHORING, TRAFFIC CONTROL, ETC.) AT NO ADDITIONAL COST TO OWNER.
 - THE DESIGNATED CONCRETE SLAB SHALL BE SHORED AS REQUIRED PRIOR TO THE START OF CONCRETE DEMOLITION. SHORING MUST BE SUPPORTED AND STAMPED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF CONNECTICUT. SHOP DRAWINGS OF THE SHORING DESIGN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO CONC. REMOVAL.
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ISOMETRIC

Drawing Title THIRD LEVEL FLOOR PLAN		STATE OF CONNECTICUT CAPITAL REGION DEVELOPMENT AUTHORITY	
Revisions		Date: APRIL 2021	
No.	Date	Description	Drawn By: AS NOTED
Project: CHURCH STREET PARKING GARAGE RENOVATIONS & IMPROVEMENTS		Project Manager: [Blank]	
Project: HARTFORD, CONNECTICUT		Project Architect: [Blank]	
CAD Number: [Blank]		Project Number: 21-016	
R-1.3			[Blank]



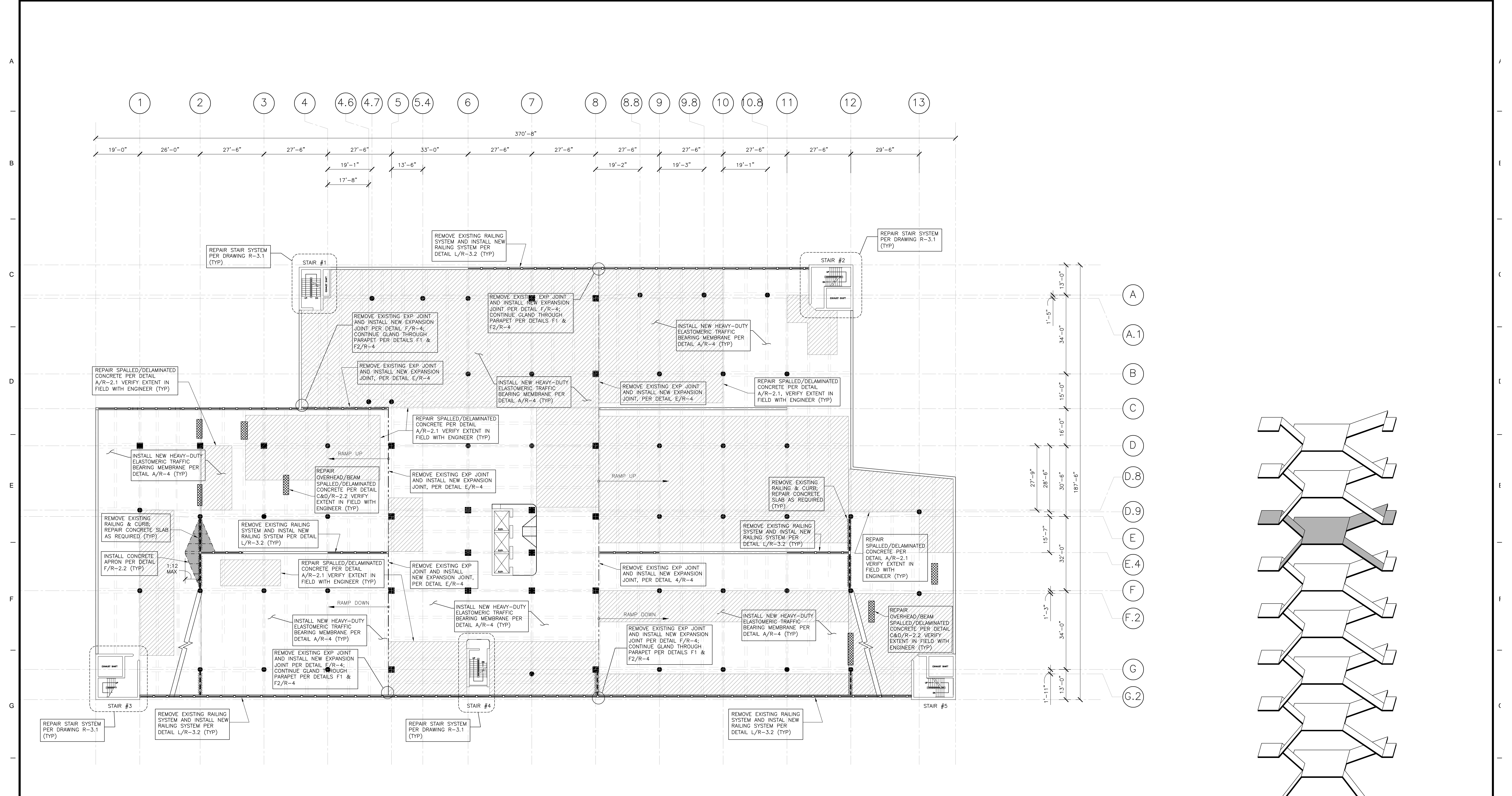
FOURTH LEVEL FLOOR PLAN
SCALE: 1/16"=1'-0"

NOTES:

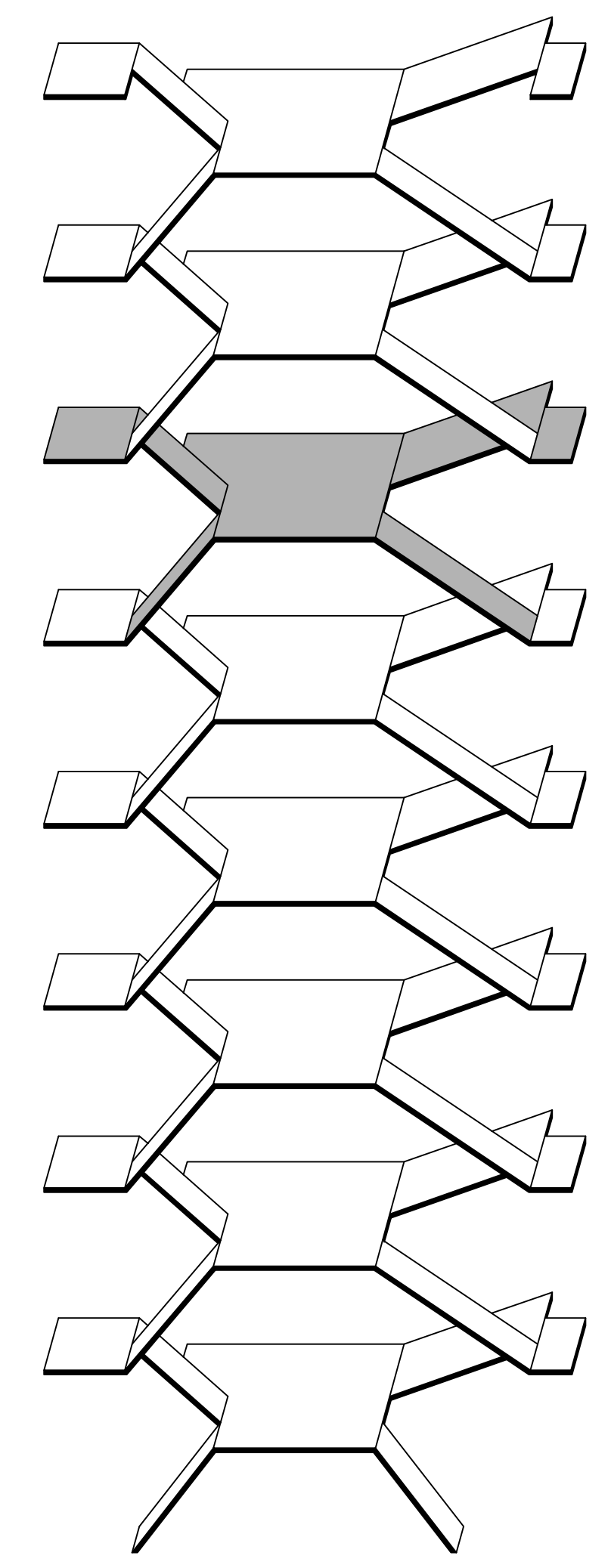
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- THE SUPPORTED CONCRETE SLAB SHALL BE SHORED AS REQUIRED PRIOR TO THE START OF CONCRETE DEMOLITION. SHORING MUST BE DESIGNED AND STAMPED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF CONNECTICUT. SHOP DRAWINGS OF THE SHORING DESIGN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO CONC. REMOVAL.
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Drawing Title FOURTH LEVEL FLOOR PLAN		STATE OF CONNECTICUT CAPITAL REGION DEVELOPMENT AUTHORITY	
Revisions		Date: APRIL 2021	
No.	Date	Description	Drawn By: AS NOTED
		Production Leader	
		Project Manager: -	
		Project: CHURCH STREET PARKING GARAGE RENOVATIONS & IMPROVEMENTS	
		HARTFORD, CONNECTICUT	
CAD Number:		Project Number: 21-016	
		R-1.4	



FIFTH LEVEL FLOOR PLAN
SCALE: 1/16"=1'-0"



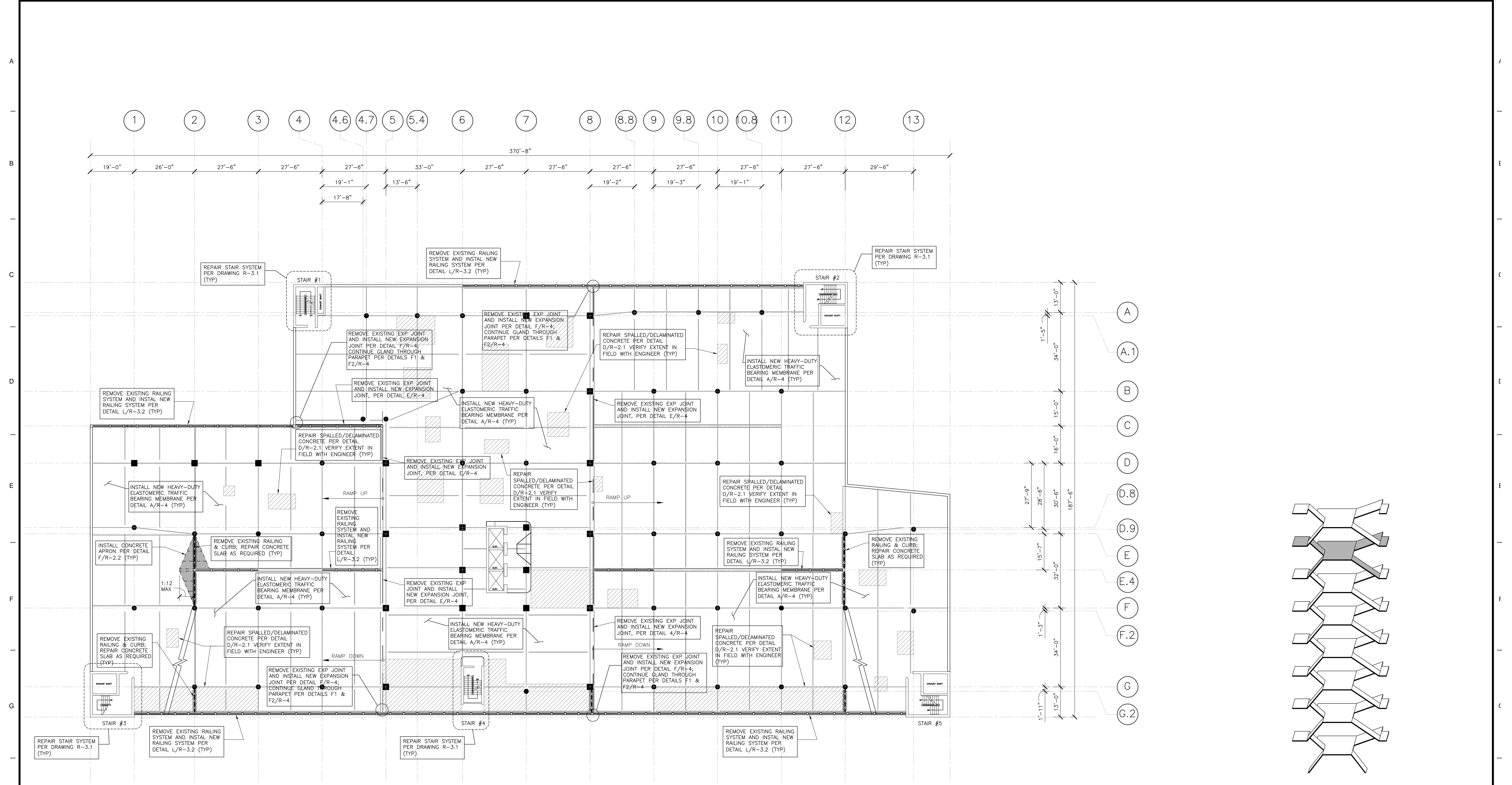
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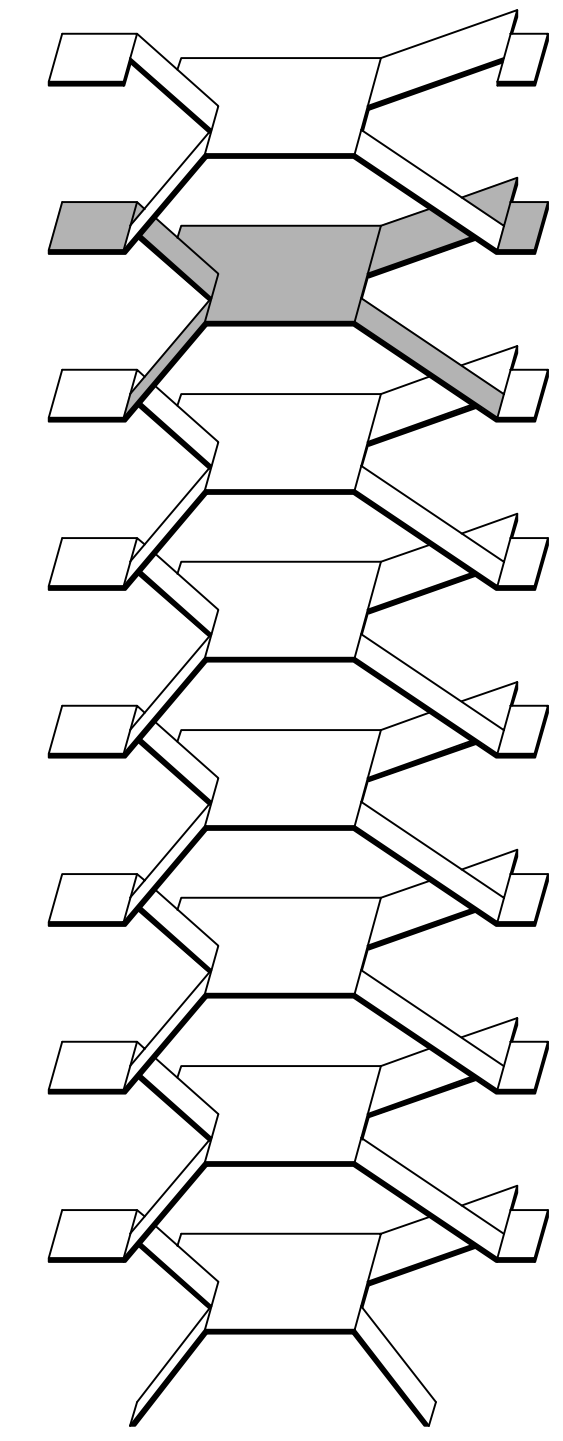
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- FOR CONCRETE DECK REPAIRS, SEE DRAWINGS R-2.1 & R-2.2. PRIOR TO START OF DEMOLITION AFFECTING A WORK AREA, CONTRACTOR SHALL LOCATE AND IDENTIFY ALL EXISTING, EMBEDDED POST-TENSION TENDONS (REGARDING THE UPPER LEVELS), BY APPROPRIATE METHODS (GROUND PENETRATING RADAR, OR APPROVED METHOD), AS APPROVED BY ENGINEER/OWNER. UPON LOCATING THE TENDONS, CONTRACTOR SHALL MARK THE LOCATIONS FOR VERIFICATION BY THE ENGINEER. DEMOLITION ACTIVITIES, AS REQUIRED FOR DESIGNATED CONCRETE REPAIRS, SHALL NOT BE STARTED UNTIL LOCATIONS OF TENDONS HAVE BEEN VERIFIED BY THE ENGINEER. IF DEMOLITION ACTIVITIES ARE STARTED BEFORE ENGINEER VERIFIES THE LOCATED TENDONS AS IDENTIFIED BY THE CONTRACTOR, CONTRACTOR SHALL PROCEED AT CONTRACTOR'S OWN RISK. SHOULD POST-TENSION TENDONS BE DAMAGED (OR ANY OTHER APPLICABLE UNFORESEEN CONDITIONS ARISE) WHILE CONTRACTOR IS PERFORMING AT CONTRACTOR'S OWN RISK, THEN CONTRACTOR SHALL REPAIR ALL DAMAGED TENDONS OR PERFORM OTHER APPLICABLE WORK (INCLUDING OF ANY COSTS ASSOCIATED WITH PROVISION OF SHORING, TRAFFIC CONTROL, ETC.) AT NO ADDITIONAL COST TO OWNER.
- THE SUPPORTED CONCRETE SLAB SHALL BE SHORED AS REQUIRED PRIOR TO THE START OF CONCRETE DEMOLITION. SHORING MUST BE DESIGNED AND STAMPED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF CONNECTICUT. SHOP DRAWINGS OF THE SHORING DESIGN SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL PRIOR TO CONC. REMOVAL.
- FOR WATERPROOFING REPAIRS, SEE DRAWING R-4. WORK SHALL INCLUDE THE INSTALLATION OF A HEAVY-DUTY, ELASTOMERIC TRAFFIC BEARING MEMBRANE, FULL-SYSTEM, MAINTAIN EXISTING CONCRETE SLOPE FOR PROPER DRAINAGE, MAKE LOCAL ADJUSTMENTS AS DEPICED ON THE DRAWINGS OR AS DIRECTED BY THE ENGINEER. IN LIMITED LOCATIONS, WORK MAY INCLUDE THE PROVISION AND INSTALLATION OF LEVELING COURSE, AS RECOMMENDED BY THE MANUFACTURER AND APPROVED BY THE ENGINEER, TO MAINTAIN POSITIVE DRAINAGE. ALL WATERPROOFING MEMBRANE INSTALLATION WORK SHALL BE BASED UPON THE HORIZONTAL PROJECTION OF THE WORK. ALL VERTICAL SURFACE APPLICATIONS (I.E. CURBING, ETC.) SHALL BE INCIDENTAL TO THE WORK. CONTRACTOR SHALL NOTE THAT ALL CRACK REPAIR, CONTROL/CONSTRUCTION JOINT REPAIR, AS WELL AS COVE JOINT REPAIR, SHALL BE INCIDENTAL TO THE APPLICATION OF THE TRAFFIC BEARING MEMBRANE, AND SHALL BE INCLUDED IN THE CONTRACT PRICE.
- WORK SHALL INCLUDE ADDRESSING ANY SHALLOW-DEPTH DEVIATIONS IN THE CONCRETE SURFACE. THESE SHALLOW-DEPTH REPAIR LOCATIONS, INCLUDING REMOVAL OF OBSTRUCTIONS TO THE LEVEL SURFACE (SUCH AS ABANDONED PIECES OF CONDUIT, FORMER ANCHORS, ETC.) SHALL TYPICALLY BE CONSIDERED ADDRESSABLE BY INSTALLATION OF A LEVELING COURSE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS; ALL LOCATIONS SHALL BE REVIEWED BY THE MANUFACTURER AND APPROVED BY THE ENGINEER PRIOR TO START OF WORK (TO CONFIRM ABANDONMENT AS APPLICABLE).

- CONTRACTOR SHALL DE-ENERGIZE AND/OR REMOVE ALL ABANDONED EMBEDDED CONDUIT IN SLAB SYSTEM AND/OR REROUTE ELECTRICAL CONDUIT AS REQUIRED TO FACILITATE CONCRETE DECK REPAIRS. THE INTENT BEING THAT EXISTING ACTIVE CONDUIT SHALL REMAIN. THEREFORE THE CONTRACTOR COORDINATE ALL STRUCTURAL, ELECTRICAL AND MECHANICAL ASPECTS OF WORK, LOCATING ALL EMBEDDED MATERIALS PRIOR TO START OF WORK. IF DEMOLITION ACTIVITIES ARE STARTED BEFORE ENGINEER VERIFIES THE LOCATED CONDUITS (OR OTHER EMBEDDED MATERIALS) AS IDENTIFIED BY THE CONTRACTOR, CONTRACTOR SHALL PROCEED AT CONTRACTOR'S OWN RISK. SHOULD CONDUITS (OR OTHER EMBEDDED MATERIALS) BE DAMAGED (OR ANY OTHER APPLICABLE UNFORESEEN CONDITIONS ARISE) WHILE CONTRACTOR IS PERFORMING AT CONTRACTOR'S OWN RISK, THEN CONTRACTOR SHALL REPAIR ALL DAMAGED CONDUITS (OR OTHER EMBEDDED MATERIALS) OR PERFORM OTHER APPLICABLE WORK (INCLUDING OF ANY COSTS ASSOCIATED WITH PROVISION OF SHORING, TRAFFIC CONTROL, ETC.) AT NO ADDITIONAL COST TO OWNER.
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- TEMPORARY LIGHTING AND/OR ELECTRICAL SERVICE TO EITHER THE WORK AREA OR OTHER AREAS IN THE FACILITY IMPACTED BY THE REPAIRS SHALL BE THE CONTRACTOR'S RESPONSIBILITY. ALL AREAS UTILIZED BY GARAGE PATRONS SHALL BE ADEQUATELY LIGHTED (MINIMUM 10 FOOT CANDLES AT CENTERLINE OF DRIVE LANES) DURING ALL PHASES OF THE WORK.
- CONTRACTOR SHALL INSTALL FULL HEIGHT PARTITIONS AROUND AND BELOW ALL WORK AREAS WHERE DEMOLITION AND SUBSEQUENT CONCRETE REPAIRS ARE BEING PERFORMED. PARTITIONS SHALL BE CONSTRUCTED TO PREVENT BOTH DUST AND FALLING DEBRIS FROM LEAVING THE CONFINES OF THE WORK AREA AS WELL AS TO PROVIDE NOISE ABATEMENT. PARTITIONS SHALL BE CONFIGURED TO ATTENUATE NOISE IN THE GARAGE, THROUGH THE USE OF QUIETED INSULATION BLANKETS, AND TO PREVENT UNAUTHORIZED ACCESS INTO ACTIVE WORK AREAS. PARTITION SYSTEMS SHALL BE CONSTRUCTED OF 2 X 4 STUDS AND A MINIMUM OF 3/8" PLYWOOD PANELS, 48" HIGH WITH PLASTIC SHEETING INSTALLED TO THE DECK ABOVE. THE PARTITION SYSTEMS SHALL BE FIRE-RATED AND ALL EXPOSED (EXTERIOR) PLYWOOD SURFACES SHALL BE PAINTED WHITE.
- THE CONTRACTOR SHALL PROVIDE MAXIMUM VEHICULAR AND PEDESTRIAN MOBILITY THROUGH AND/OR IN THE VICINITY OF THE PROJECT AREA. PROPER COORDINATION BETWEEN THE CONTRACTOR AND GARAGE MANAGEMENT STAFF SHALL BE REQUIRED IN THE SEQUENCING AND SCHEDULING OF REPAIRS. MOVEMENT OF MATERIAL, CONSTRUCTION DEBRIS AND THE REMEDIATION OF NEW WORK AREAS SHALL NOT TAKE PLACE DURING PERIODS OF PEAK TRAFFIC FLOW. BOTH VEHICULAR AND PEDESTRIAN TRAFFIC SHALL BE MAINTAINED AT ALL TIMES, AND CONTRACTOR SHALL INSTALL TRAFFIC CONTROL SIGNS AND LIGHTS FOR THE SAFE PASSAGE THROUGH THE GARAGE, THROUGH THE STAIRS, AND AT THE ENTRANCES AND EXITS DURING THE ENTIRE CONSTRUCTION PERIOD. ONLY ONE STAIR AND/OR ONE ELEVATOR MAY BE TAKEN OUT OF SERVICE AT ANY GIVEN TIME BUT NEVER BOTH AT THE SAME STAIR/ELEVATOR TOWER, PROPERLY COORDINATED WITH OWNER IN ADVANCE. SIGNAGE MAY BE REQUIRED ON THE SURROUNDING STREETS IF REPAIR WORK IS TO BE ACCOMPLISHED ON THE ENTRY/EXIT RAMP. CONTRACTOR IS TO OBTAIN PERMITS FROM APPROPRIATE AGENCIES.
- FOLLOWING COMPLETION OF MEMBRANE WORK, CONTRACTOR SHALL APPLY NEW MARKINGS FOR ALL PARKING STALLS, DIRECTIONAL ARROWS, CROSSWALKS, CURBS, CENTERLINES, TEXT AND OTHER TRAFFIC MARKINGS LOCATED THROUGHOUT THE GARAGE. APPLICATION OF STRIPING SHALL TYPICALLY BE PERFORMED TO MATCH PRE-EXISTING CONDITIONS OR AS OTHERWISE DIRECTED BY THE ENGINEER. HOWEVER ALL HANDICAP PARKING STALLS SHALL BE UPDATED ACCORDINGLY PER CURRENT REQUIREMENTS AND QUANTITY/LOCATION OF HANDICAP PARKING STALLS SHALL BE VERIFIED PRIOR TO START OF WORK; ALL CURBINGS SHALL BE PAINTED, BOTH THOSE CURRENTLY PAINTED AND THOSE NOT. ANY AND ALL CONFLICTING MARKINGS SHALL BE REMOVED IN THEIR ENTIRETY BY APPROVED METHOD. OWNER/ENGINEER SHALL BE REQUIRED TO REVIEW STRIPING LAYOUT PRIOR TO THIS WORK BEING PERFORMED.

FIFTH LEVEL FLOOR PLAN Revisions:		STATE OF CONNECTICUT CAPITAL REGION DEVELOPMENT AUTHORITY	
No. Date Description	Plans Prepared By:	DESMAN Design Management	Date: APRIL 2021 Scale: AS NOTED Production Leader:
Project:		Project Manager:	
CHURCH STREET PARKING GARAGE RENOVATIONS & IMPROVEMENTS		Project Architect:	
HARTFORD, CONNECTICUT		Peer Review:	
CAD Number:		Drawing Number:	
Project Number: 21-016		R-15	



SIXTH LEVEL FLOOR PLAN
SCALE: 1/16"=1'-0"

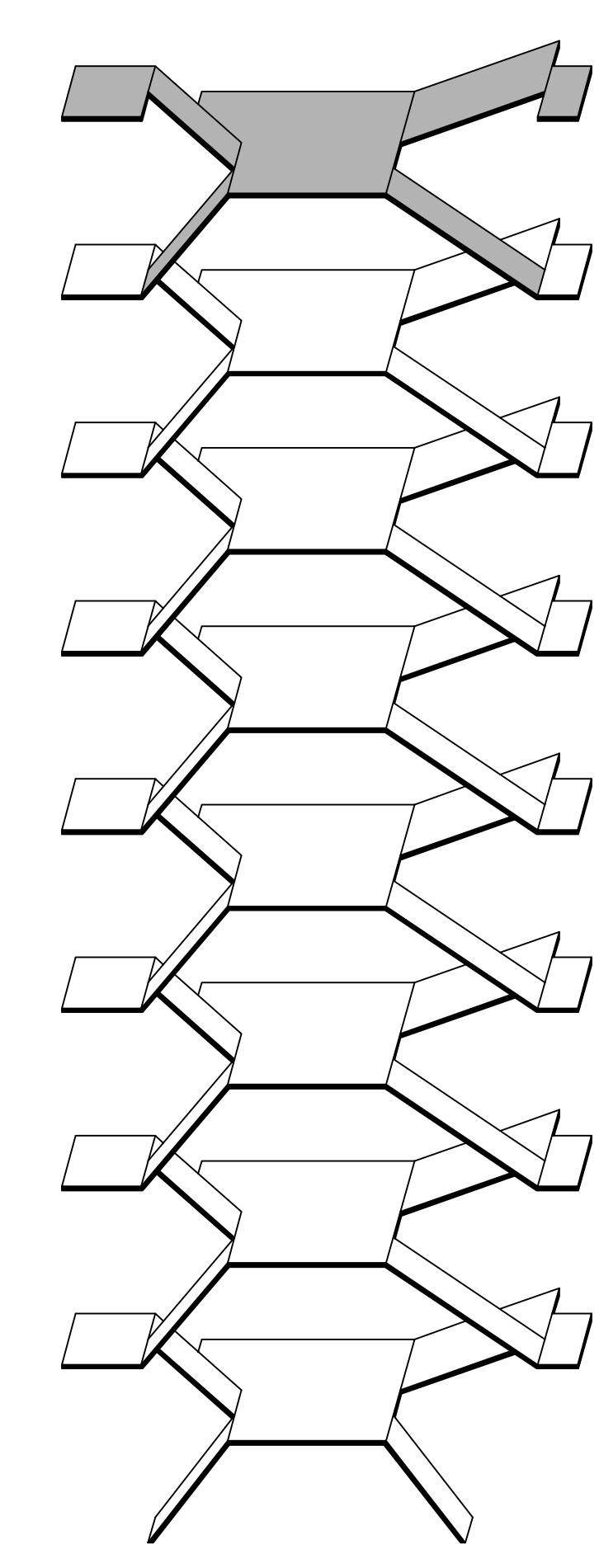
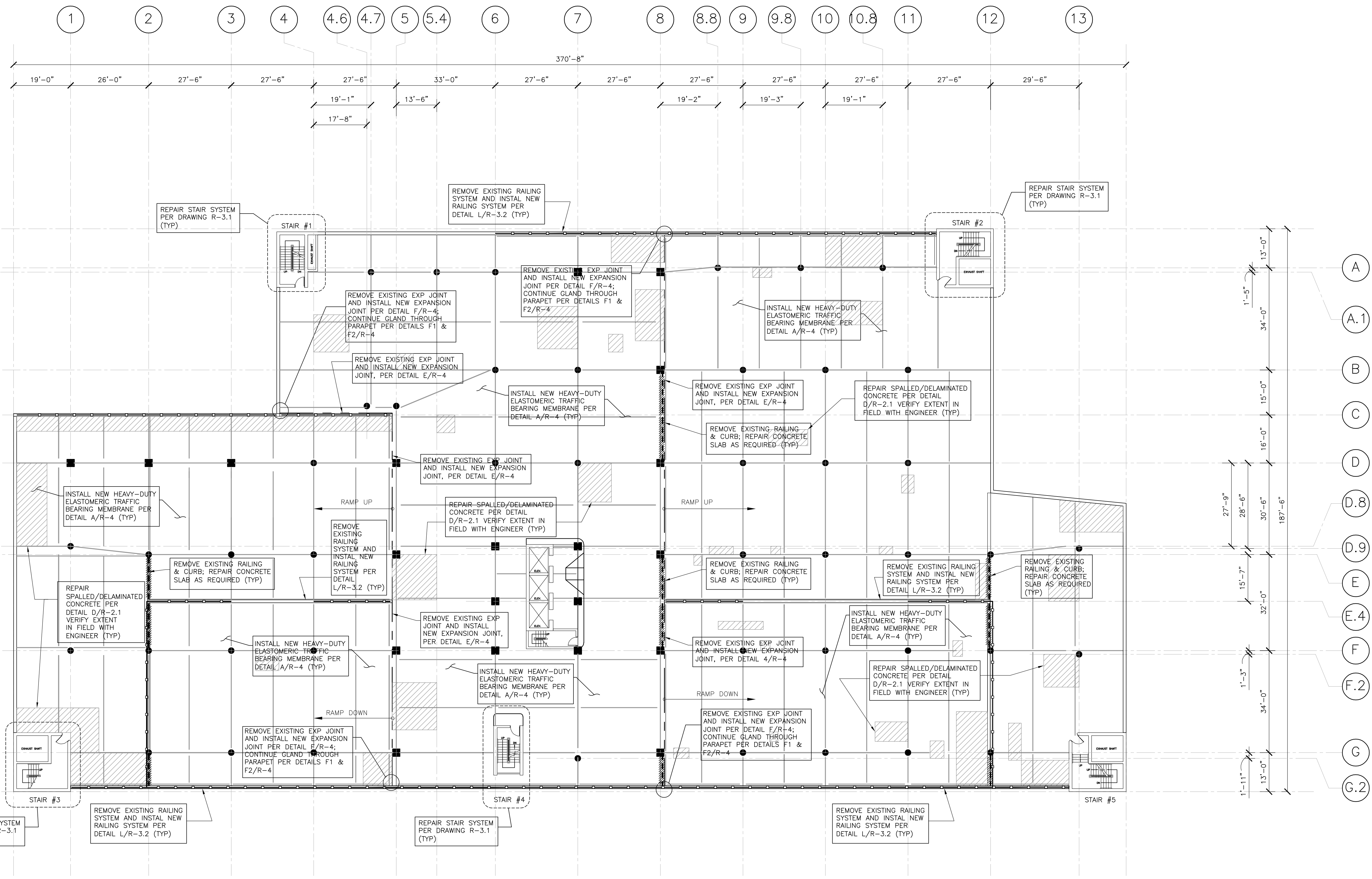


ISOMETRIC

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Drawing Title SIXTH LEVEL FLOOR PLAN		STATE OF CONNECTICUT CAPITAL REGION DEVELOPMENT AUTHORITY	
Revisions		Date: APRIL 2021	
No.	Date	Description	Drawn By: AS NOTED
			Production Leader
Project: CHURCH STREET PARKING GARAGE RENOVATIONS & IMPROVEMENTS		Project Manager:	
HARTFORD, CONNECTICUT		Project Architect:	
CAD Number:		Drawing Number: R-1.6	
Project Number: 21-016			



SEVENTH (ROOF) LEVEL FLOOR PLAN
 SCALE: 1/16"=1'-0"

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SEVENTH (ROOF) LEVEL FLOOR PLAN Revisions:		STATE OF CONNECTICUT CAPITAL REGION DEVELOPMENT AUTHORITY	
No. Date Description	Plans Prepared By:		Date: APRIL 2021 Scale: AS NOTED Project Manager:
		Project: CHURCH STREET PARKING GARAGE RENOVATIONS & IMPROVEMENTS HARTFORD, CONNECTICUT	Project Number: 21-016 Drawing Number: R-1.7

CONSTRUCTION NOTES:

- CODES AND STANDARDS: ALL REPAIRS SHALL BE PERFORMED IN CONFORMANCE WITH THE PERTINENT PROVISIONS OF THE FOLLOWING CODES AND STANDARDS:
 - ACI 304 RECOMMENDED PRACTICE FOR MEASURING, MIXING, TRANSPORTING AND PLACING CONCRETE.
 - ACI 305 RECOMMENDED PRACTICE FOR HOT WEATHER CONCRETING.
 - ACI 306 RECOMMENDED PRACTICE FOR COLD WEATHER CONCRETING.
 - ACI 309 GUIDE FOR CONSOLIDATION OF CONCRETE.
 - ACI 311 GUIDE FOR CONCRETE INSPECTION.
 - ACI 318 BUILDING CODE REQUIREMENTS FOR REINFORCED CONCRETE.
 - ACI 347 GUIDE TO FORMWORK FOR CONCRETE.
 - AISC CODE OF PRACTICE FOR STEEL BUILDINGS AND BRIDGES.
 - AISC SPECIFICATIONS FOR THE DESIGN, FABRICATION AND ERECTION OF STRUCTURAL STEEL FOR BUILDINGS.
 - AISC SPECIFICATIONS FOR STRUCTURAL JOINTS USING ASTM A325 OR A490 BOLTS.
 - AWS D1.1 STRUCTURAL WELDING CODE.
 - ASTM APPROPRIATE SPECIFICATIONS OF THE AMERICAN SOCIETY FOR TESTING AND MATERIALS.

THE ENGINEER SHALL HAVE THE AUTHORITY TO RETEST THE WORK WHICH DOES NOT CONFORM TO THE CONTRACT DOCUMENTS. THE ENGINEER WILL HAVE THE AUTHORITY TO REQUIRE SPECIAL INSPECTION OR TESTING OF THE WORK.

NEITHER, HOWEVER, THE ENGINEER'S AUTHORITY TO ACT UNDER THIS SUBPARAGRAPH NOR ANY DECISION MADE BY HIM, IN GOOD FAITH, EITHER TO EXERCISE OR NOT TO EXERCISE SUCH AUTHORITY, SHALL GIVE RISE TO ANY RESPONSIBILITY OF THE ENGINEER TO THE CONTRACTOR, SUBCONTRACTORS, AGENTS, EMPLOYEES OR ANY OTHER PERSON PERFORMING ANY OF THE WORK.

THE CONTRACTOR SHALL MAINTAIN, ON SITE ONE COMPLETE SET OF DRAWINGS (WHITE PRINTS) AND SPECIFICATIONS, FURNISHED BY THE OWNER, AN ACCURATE RECORD OF THE INSTALLATION OF ALL MATERIALS AND SYSTEMS COVERED BY THE CONTRACT. THE RECORD DRAWINGS AND SPECIFICATIONS SHALL INDICATE THE EXACT LOCATION AND AMOUNT OF ALL REPAIR WORK. THIS COMPLETE SET OF RECORD DRAWINGS AND SPECIFICATIONS MUST BE DELIVERED TO THE ENGINEER AS SOON AS THE GARAGE REPAIR PROJECT IS SUBSTANTIALLY COMPLETE. FINAL PAYMENT WILL BE WITHHELD UNTIL DELIVERY OF AN ACCEPTABLE SET OF RECORD DOCUMENTS HAS BEEN MADE.

THE CONSTRUCTION SCHEDULE SHALL BE COORDINATED WITH BOTH THE ENGINEER AND OWNER. PRELIMINARY CONSTRUCTION SCHEDULE IS REQUIRED WITHIN TEN CALENDAR DAYS FROM NOTICE TO PROCEED.

THE CONTRACTOR SHALL INSTALL TEMPORARY DUSTPROOF AND NOISE ABATING PARTITIONS ALONG THE PERIPHERY OF THE WORK AREA. THESE PARTITIONS SHALL BE DESIGNED TO ATTENUATE NOISE AND TO PREVENT UNAUTHORIZED ACCESS INTO THE WORK AREA. THE PARTITIONS SHALL PROTECT OTHER AREAS WITHIN THE FACILITY FROM WATER, DUST AND FLYING DEBRIS. THE CONTRACTOR SHALL PROVIDE A DUST COLLECTION/FILTER SYSTEM TO PREVENT DUST AND DEBRIS FROM ESCAPING IN THE WORK AREA VENTILATION EXHAUST. ALL DRAINAGE SYSTEMS SHALL BE PROTECTED FROM DAMAGE AND SHALL BE MAINTAINED IN FUNCTIONING ORDER.

THE CONTRACTOR IS RESPONSIBLE FOR PHASING HIS WORK TO ALLOW ACCESS INTO AND OUT OF THE GARAGE PORTIONS THAT ARE OPERATIONAL DURING THE CONSTRUCTION PERIOD. THE RESPONSIBILITY INCLUDES, BUT IS NOT LIMITED TO PROVIDING ADDITIONAL PERSONNEL (FLAGMEN), POLICE OFFICERS AND TRAFFIC CONTROL DEVICES AS RECOMMENDED BY THE UNIFORM TRAFFIC CODE OR AS ACCEPTABLE TO THE ENGINEER/OWNER.

THE CONTRACTOR SHALL PAY SPECIAL ATTENTION TO THE NOISE LEVELS GENERATED BY THE CONCRETE REMOVAL OPERATIONS AND SHOTBLASTING/BLAST TRACKING OF THE DECK PRIOR TO WATERPROOFING/MEMBRANE/SEALER APPLICATION. OBEY ALL APPLICABLE NOISE LAWS. CONTRACTOR SHALL ALSO USE NOISE DAMPENING/SOUND ABSORBING INSULATION IN PERIMETER PARTITIONS TO ABATE NOISE TO NEARBY FACILITIES.

THE CONTRACTOR IS RESPONSIBLE FOR REVIEWING THE UNDERSIDE OF THE SUPPORTED DECKS NOTING ALL LOOSE MATERIAL WHICH MIGHT BE A POTENTIAL HAZARD IF ABATED DURING DEMOLITION AND PREVENTING ANY SUCH MATERIAL FROM FALLING.

THE NEW CONCRETE SHALL BE PLACED, CONSOLIDATED AND FINISHED AS SPECIFIED. THE NEWLY REPAIRED AREAS MAY BE OPENED TO GARAGE OPERATION (TO FACILITATE TRAFFIC FLOW AND ALLOW PARKING) AFTER THE NEW CONCRETE HAS ACHIEVED A MINIMUM COMPRESSIVE STRENGTH OF 5750 PSI. THE CONTRACTOR SHALL COOPERATE WITH THE OWNER'S TESTING AGENCY REPRESENTATIVE DURING CONCRETE PLACEMENT AND ASSIST IN MAKING TEST CYLINDERS OF CONCRETE DELIVERED TO THE SITE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ADEQUATELY SHORING AND SHEETING EXISTING/ ADJACENT STRUCTURES AND FACILITIES AS MAY BE REQUIRED, SO THAT THEY ARE NOT ENDANGERED BY THE NEW CONSTRUCTION.

REINFORCED CONCRETE NOTES:

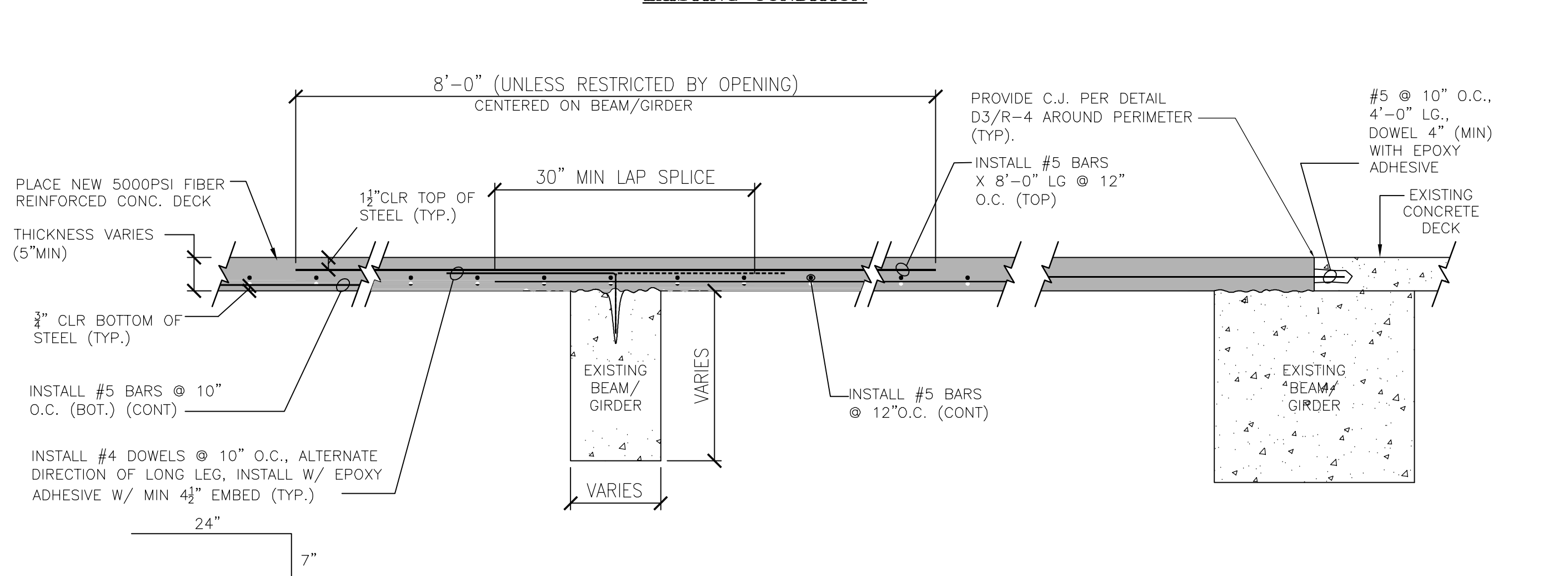
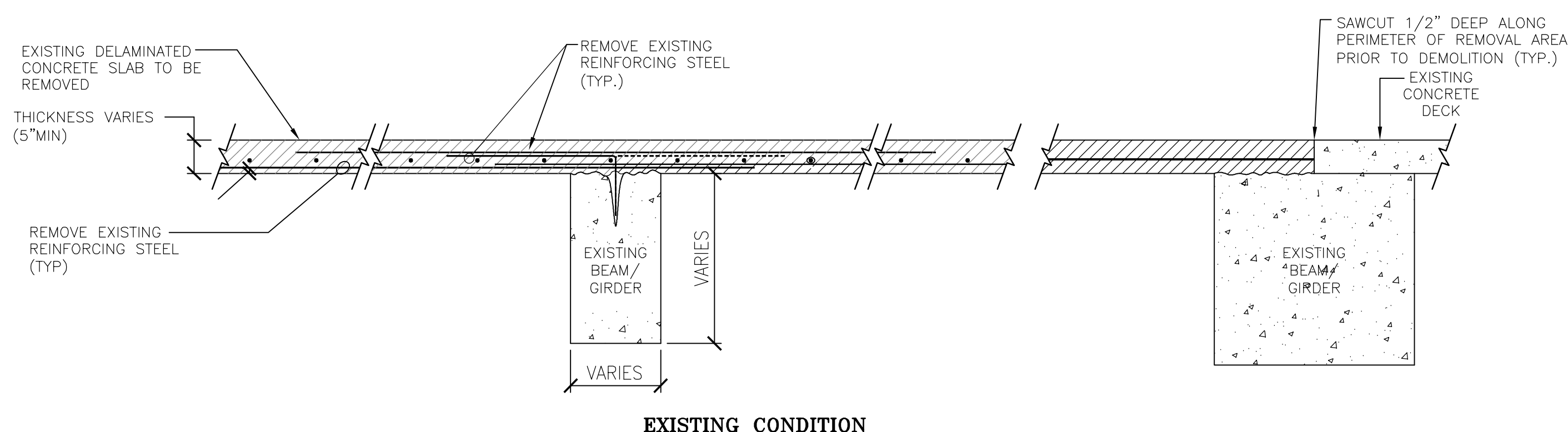
- ALL REINFORCED CONCRETE WORK SHALL BE IN ACCORDANCE WITH "BUILDING CODE" REQUIREMENTS FOR REINFORCED CONCRETE ACI 318.
- MATERIALS:
 - THE 28 DAY COMPRESSIVE STRENGTH OF CONCRETE REPAIR MATERIAL SHALL BE A MINIMUM OF 5,000 PSI. THE CONCRETE SHALL BE NORMAL WEIGHT, FIBER REINFORCED CONCRETE IN ACCORDANCE WITH THE SPECIFICATIONS, UNLESS NOTED OTHERWISE.
 - THE CONCRETE MIX DESIGN SHALL BE PROPORTIONED AND TESTED BY AN APPROVED TESTING LABORATORY. MIX DESIGNS AND TEST DATA SHALL BE SUBMITTED TO THE OWNER AND ENGINEER FOR APPROVAL BEFORE USE.
 - THE COARSE AGGREGATE SHALL BE CLEAN SOUND AGGREGATE MEETING THE REQUIREMENTS OF ASTM C33.
 - THE SAND (FINE AGGREGATE) SHALL BE CLEAN AND SHARP MEETING THE REQUIREMENTS OF ASTM C33.
 - THE PORTLAND CEMENT SHALL BE TYPE I OR III CONFORMING TO ASTM C150.
 - ALL NEW REINFORCING STEEL SHALL CONFORM TO ASTM A615 (60,000 PSI YIELD).
 - ALL NEW WELDED PLAIN WIRE FABRIC SHALL CONFORM TO ASTM A185 (60,000 PSI YIELD).
 - AIR ENTRAINMENT FOR ALL CONCRETE SHALL BE 5.5%-7%; 1% OF THE VOLUME OF THE FRESHLY MIXED CONCRETE.
- ALL DIMENSIONS SHOWN FOR LOCATION OF REINFORCING STEEL ARE TO THE FACE OF BARS AND DENOTE MINIMUM CLEAR COVER. UNLESS SPECIFICALLY NOTED, CONCRETE COVER OVER NEW OR SUPPLEMENTAL STEEL SHALL BE AS FOLLOWS:
 - 1-1/2" FOR TOP BARS.
 - 3/4" FOR BOTTOM BARS IN SUPPORTED SLABS.
 - 3/4" FOR ALL EXISTING REINFORCING STEEL (TOP AND BOTTOM).

WITHIN CONCRETE REPAIR AREAS, THE MINIMUM CLEAR COVER ABOVE EXISTING REINFORCING STEEL SHALL MEET OR EXCEED THE ABOVE MENTIONED REQUIREMENTS TO THE GREATEST EXTENT POSSIBLE. IF CONDITIONS DO NOT ALLOW THE PROVISION OF THE MINIMUM CLEAR COVER, THE CONTRACTOR SHALL CONSULT WITH THE ENGINEER FOR SPECIFIC CORRECTIVE ACTION.

ALL REINFORCING STEEL DETAILS SHALL BE IN ACCORDANCE WITH THE MANUAL OF STANDARD PRACTICE FOR DETAILING REINFORCED CONCRETE STRUCTURES, ACI LATEST EDITION.

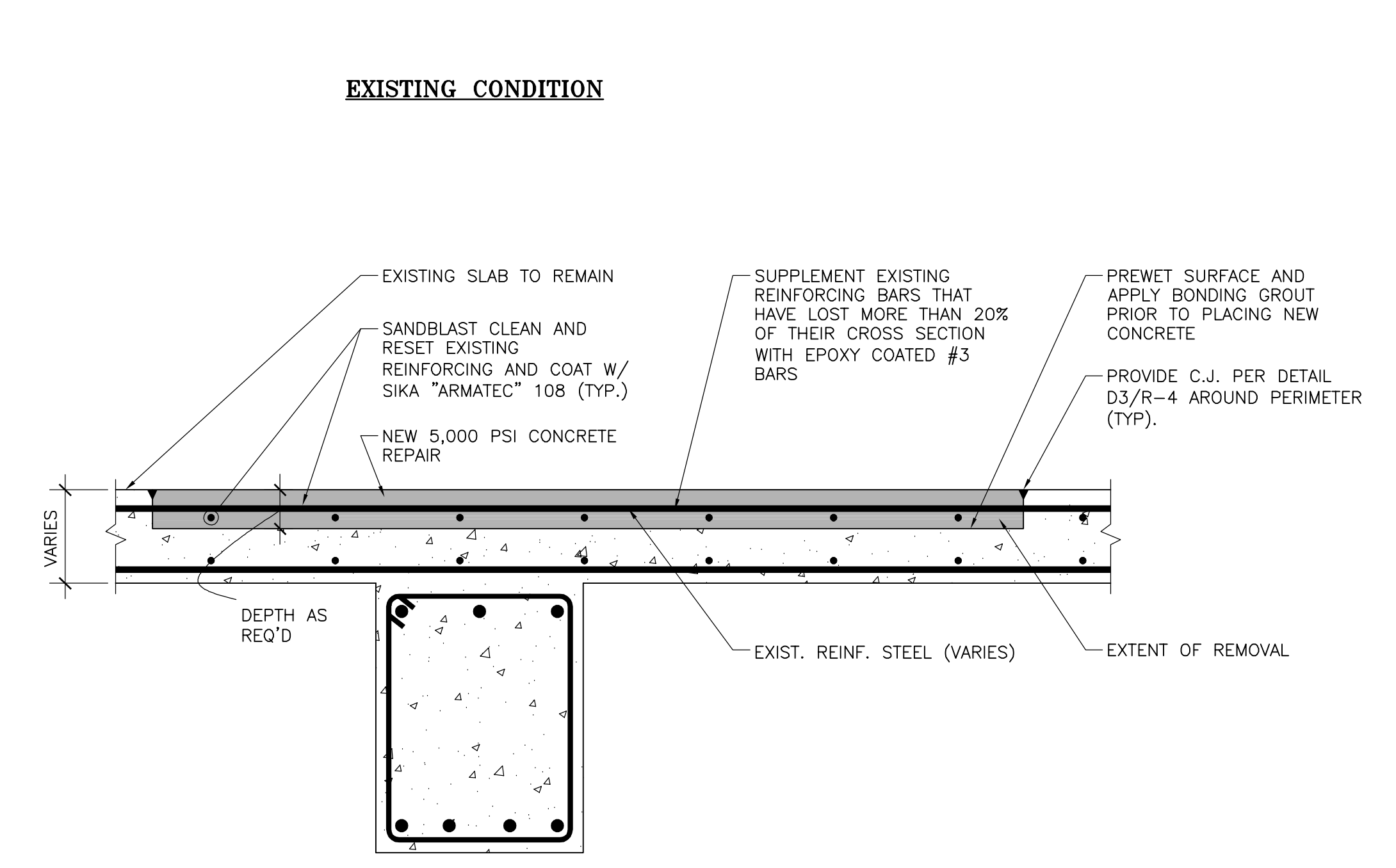
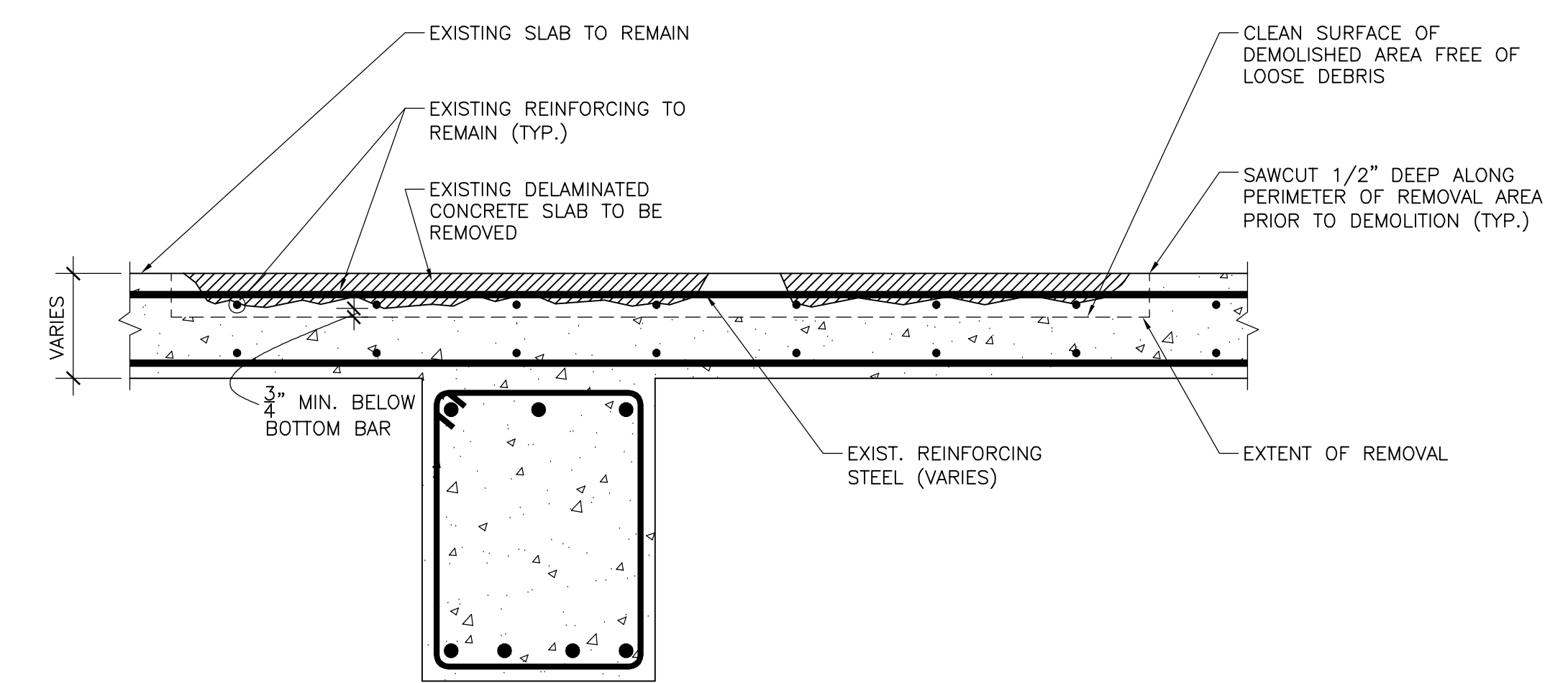
THE REPAIR AREAS SHALL BE FINISHED AS NOTED IN THE SPECIFICATION, AND IN A MANNER ACCEPTABLE TO THE WATERPROOFING MEMBRANE MANUFACTURER. ALL CONCRETE REPAIR AREAS SHALL BE PROPERLY PREPARED AROUND THEIR PERIMETER AND SEALED WITH AN APPROVED SEALANT. ALL CONCRETE REPAIR AREAS WHICH HAVE EXISTING CONSTRUCTION JOINTS RUNNING INTO THEM SHOULD HAVE A JOINT TOOLED THROUGH THEM TO BE SEALED.

- CONCRETE REPAIR PROCEDURE:**
- INSTALL TRAFFIC CONTROL SIGNS AND DEVICES NECESSARY TO REROUTE THE TRAFFIC. PROVIDED TRAFFIC SIGNS AND LIGHTS FOR THE SAFE VEHICULAR PASSAGE THROUGH THE GARAGE AND AT THE ENTRANCES AND EXITS DURING ENTIRE CONSTRUCTION PERIOD. SIGNAGE MAY BE REQUIRED ON THE SURROUNDING STREETS IF REPAIR WORK IS TO BE ACCOMPLISHED NEAR THE ENTRY/EXITS. CONTRACTOR TO OBTAIN PERMITS FROM APPROPRIATE AGENCIES.
 - THE DECK SHALL BE SHORED AS REQUIRED PRIOR TO CONCRETE DEMOLITION. SHORING TO BE DESIGNED BY THE CONTRACTOR TO CARRY A MIN. OF 150 PSF CONSTRUCTION LOAD AND ALL CONCENTRATED LOADING IMPOSED BY THE DEMOLITION EQUIPMENT. THE CONTRACTOR SHALL SUBMIT SHORING DRAWINGS DESIGNED AND STAMPED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF CONNECTICUT FOR APPROVAL PRIOR TO COMMENCING ANY WORK.
 - ERECT FULL-HEIGHT PARTITIONS AROUND & BELOW THE WORK AREA TO PREVENT DUST & DEBRIS FROM LEAVING THE WORK AREA.
 - REMOVE CONCRETE TO SPECIFIED DEPTH USING DEMOLITION PROCEDURES IN ACCORDANCE WITH THE SPECIFICATIONS.
 - VERIFY, WITH ENGINEER, THE EXACT PERIMETER OF CONCRETE TO BE REMOVED, AND SAWCUT PERIMETER OF THE AREA TO BE REPAIRED REMOVE EXISTING REINFORCING STEEL AND REPLACE WITH NEW. THAT STEEL WHICH REMAINS WITHOUT CUTTING REINFORCING STEEL, SANDBLAST CLEAN THE EXISTING REINF. AND SUPPLEMENT REINFORCING BARS THAT HAVE LOST 20% OR MORE OF THEIR CROSS-SECTIONAL AREA WITH NEW REINFORCING STEEL UNLESS OTHERWISE DIRECTED BY THE ENGINEER.
 - THE EXISTING CONCRETE SUBSTRATE SHALL BE SATURATED AND SURFACE DRIED PRIOR TO BONDING COATING. APPLY BONDING GROUT IMMEDIATELY PRIOR TO THE CONCRETE PLACEMENT.
 - FORM, PLACE, FINISH AND CURE WITH LOW W/C RATIO FIBER REINFORCED CONCRETE AS SHOWN AND IN ACCORDANCE WITH THE SPECIFICATIONS. PROVIDE TOOLED JOINTS AS SHOWN ON THE DRAWINGS AND/OR AS SPECIFIED BY THE ENGINEER.

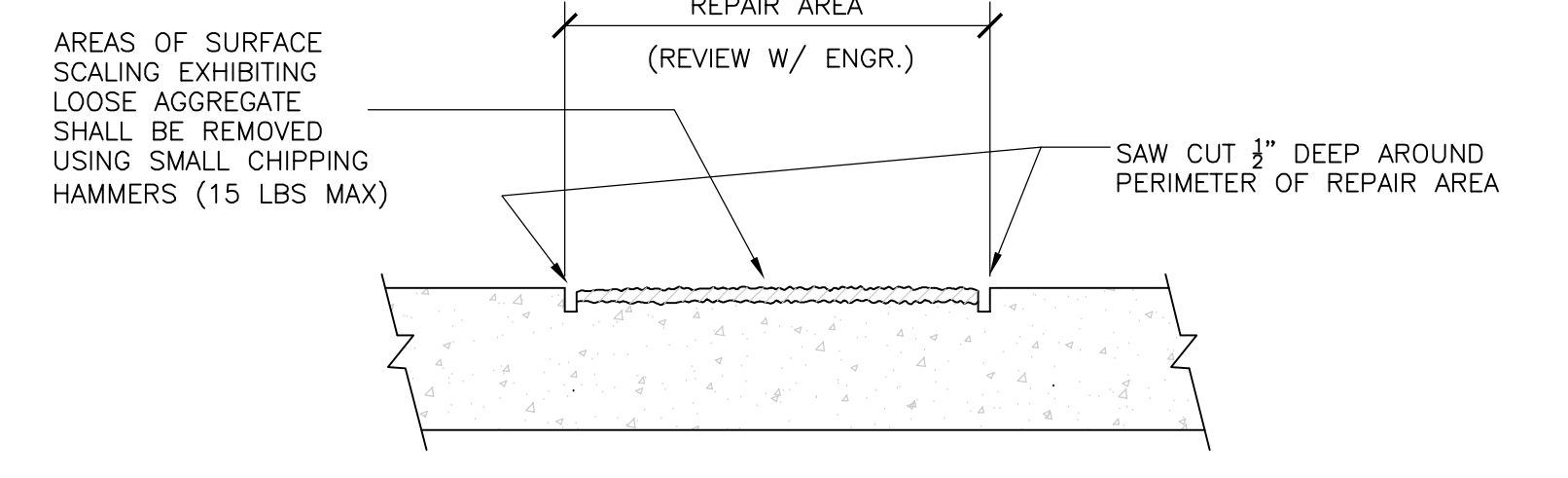
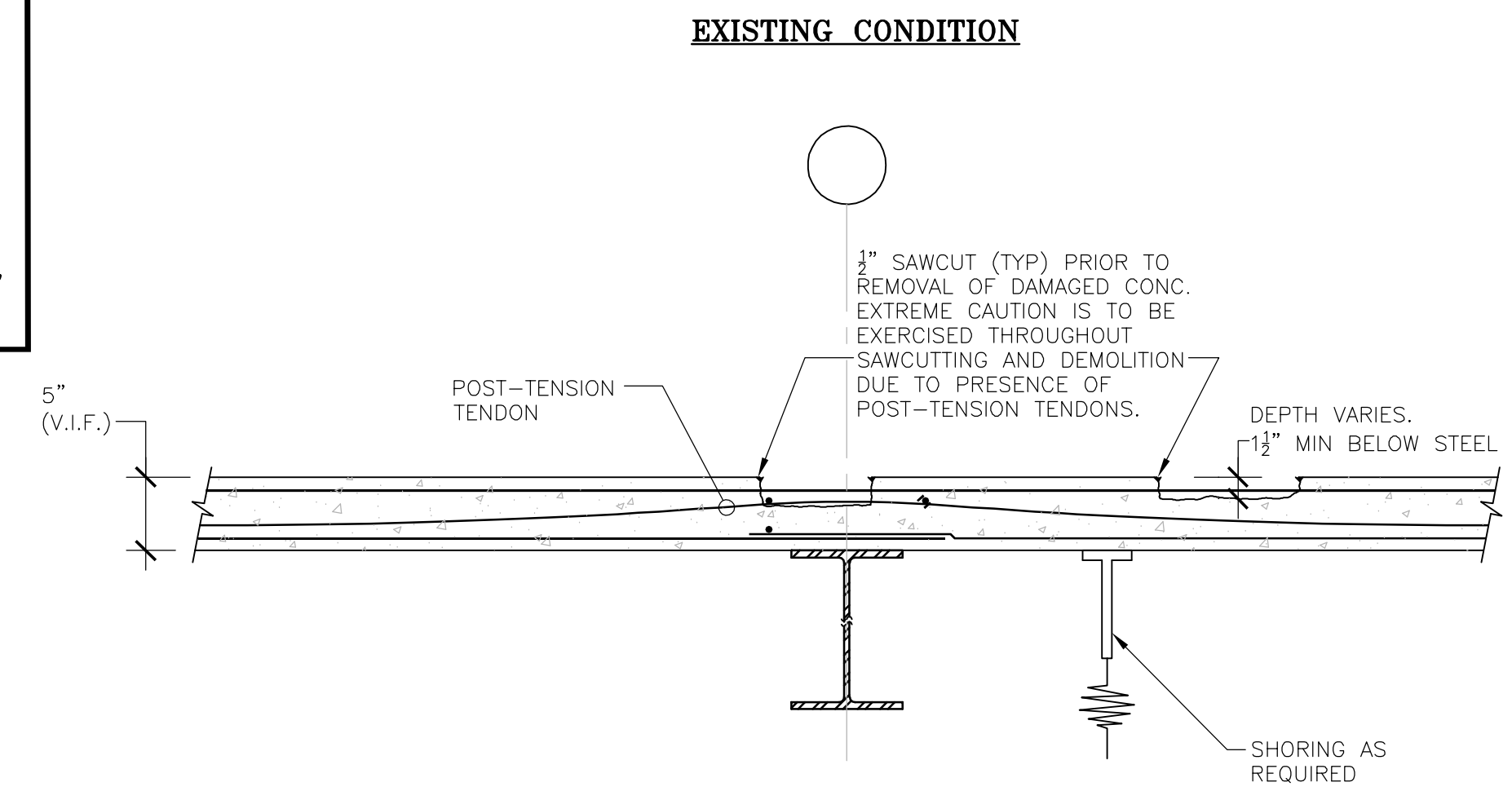
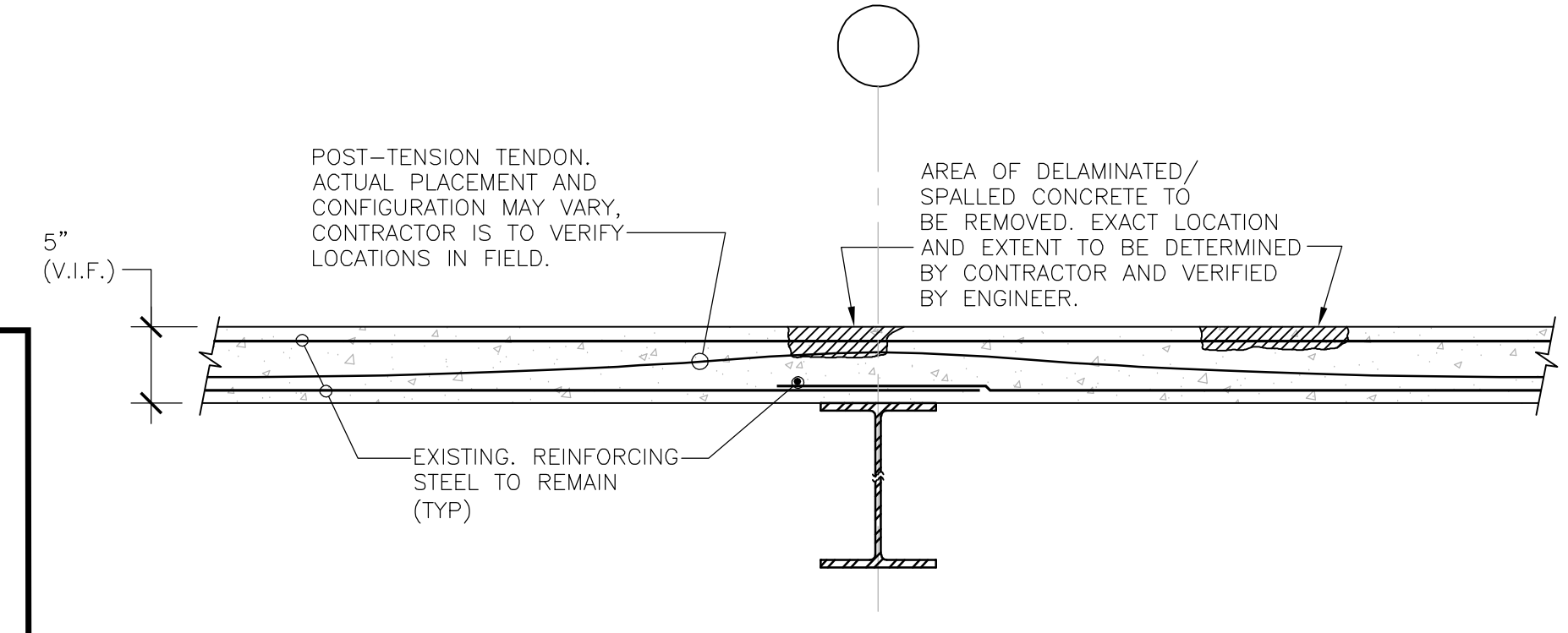


- NOTE:
- BEAM/GIRDER REINFORCEMENT NOT SHOWN FOR CLARITY
 - REMOVE ALL EXISTING REINFORCING STEEL; INSTALL NEW STEEL IN ACCORDANCE WITH SPECIFICATIONS.

A FULL-DEPTH CONCRETE REPAIR
R-2.1 SCALE: N.T.S.



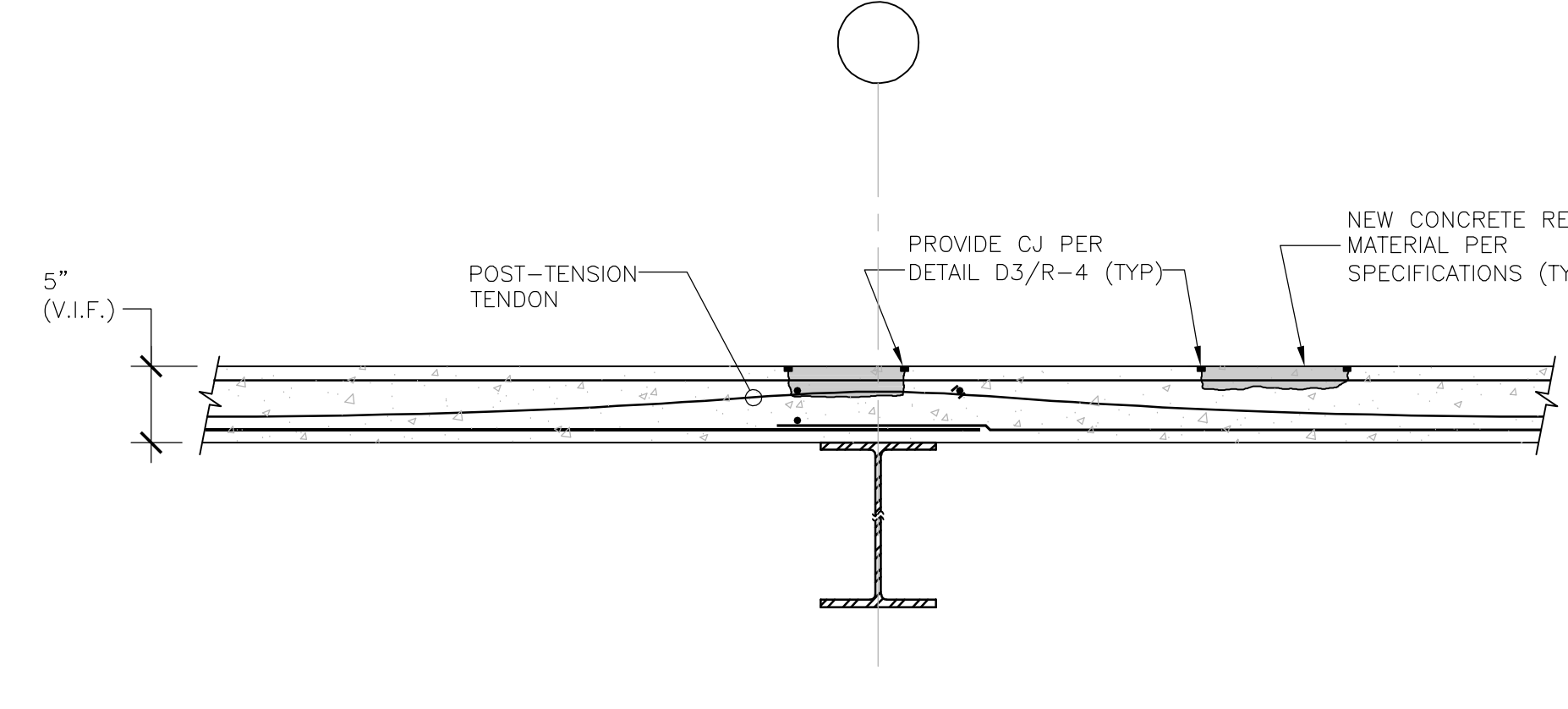
B PARTIAL-DEPTH CONCRETE REPAIR
R-2.1 SCALE: N.T.S.



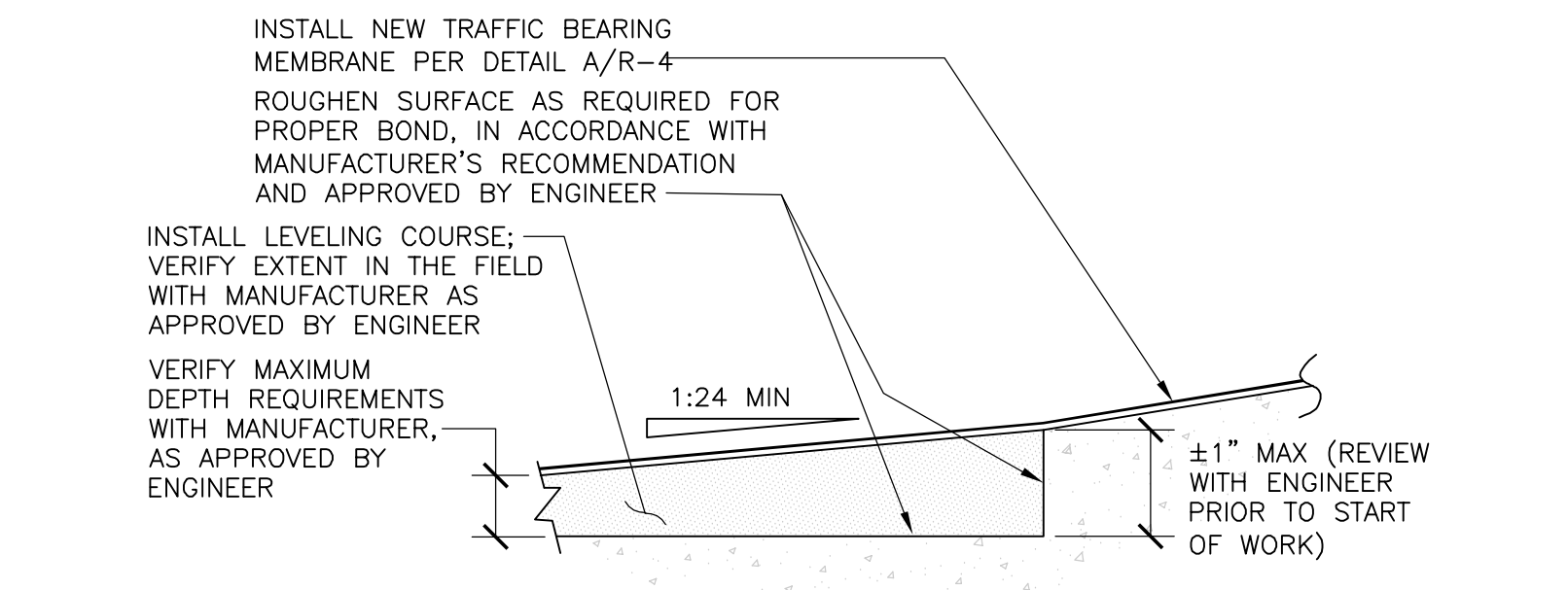
- REPAIR PROCEDURE:**
- AFTER REMOVAL OF LOOSE/DETERIORATED CONCRETE SCALING AREAS, THE REPAIR AREA SHALL BE SHOTBLASTED TO REMOVE OIL, DIRT, RUBBER OR OTHER POTENTIALLY DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND OR LATANCES WHICH, IN THE MANUFACTURER'S OR ENGINEER'S OPINION, PREVENT PROPER BONDING AND CURING OF THE APPROVED PATCH MATERIAL.
 - REPAIR AREA SHALL BE DRY AT THE TIME OF APPLICATION OF APPROVED PATCH MATERIAL. IMMEDIATELY PRIOR TO MATERIAL INSTALLATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR AND/OR VACUUMED TO REMOVE DUST AND DEBRIS.
 - APPROVED PATCH MATERIAL SHALL BE APPLIED IN STRICT ACCORDANCE WITH MANUFACTURER'S INSTALLATION INSTRUCTIONS AS APPROVED BY THE ENGINEER. CERTIFIED MANUFACTURER'S REPRESENTATIVE SHALL BE REQUIRED TO OBSERVE SURFACE PREPARATION AND MATERIAL INSTALLATION FOR APPROXIMATE 25% OF TOTAL REPAIR AREA, AND SHALL PROVIDE WRITTEN VERIFICATION THAT PROPER PROCEDURES HAVE BEEN FOLLOWED.
 - APPROVED AGGREGATE SHALL BE BROADCAST OVER THE REPAIR AREAS TO REFUSAL (I.E. NO WET AREAS SHALL SHOW). AGGREGATE SHALL BE DISTRIBUTED IN SUCH A MANNER THAT THE LEVEL OF REPAIR MATERIAL IS NOT DISTURBED.

NOTE:
CONTRACTOR SHALL REVIEW ALL LOCATIONS OF SHALLOW-DEPTH/LEVELING REPAIR WITH MANUFACTURER PRIOR TO START OF WORK. CONTRACTOR SHALL CONFIRM COMPATIBILITY OF REPAIR MATERIALS WITH MEMBRANE SYSTEM MATERIALS, AND SHALL PROVIDE WRITTEN CONFIRMATION FROM THE MEMBRANE MANUFACTURER OF COMPATIBILITY FOR REVIEW BY OWNER/ENGINEER. REFERENCE SPECIFICATION SECTION 071200 FOR ADDITIONAL INFORMATION.

C SHALLOW-DEPTH/SCALING/LEVELING REPAIR
R-2.1 SCALE: N.T.S.

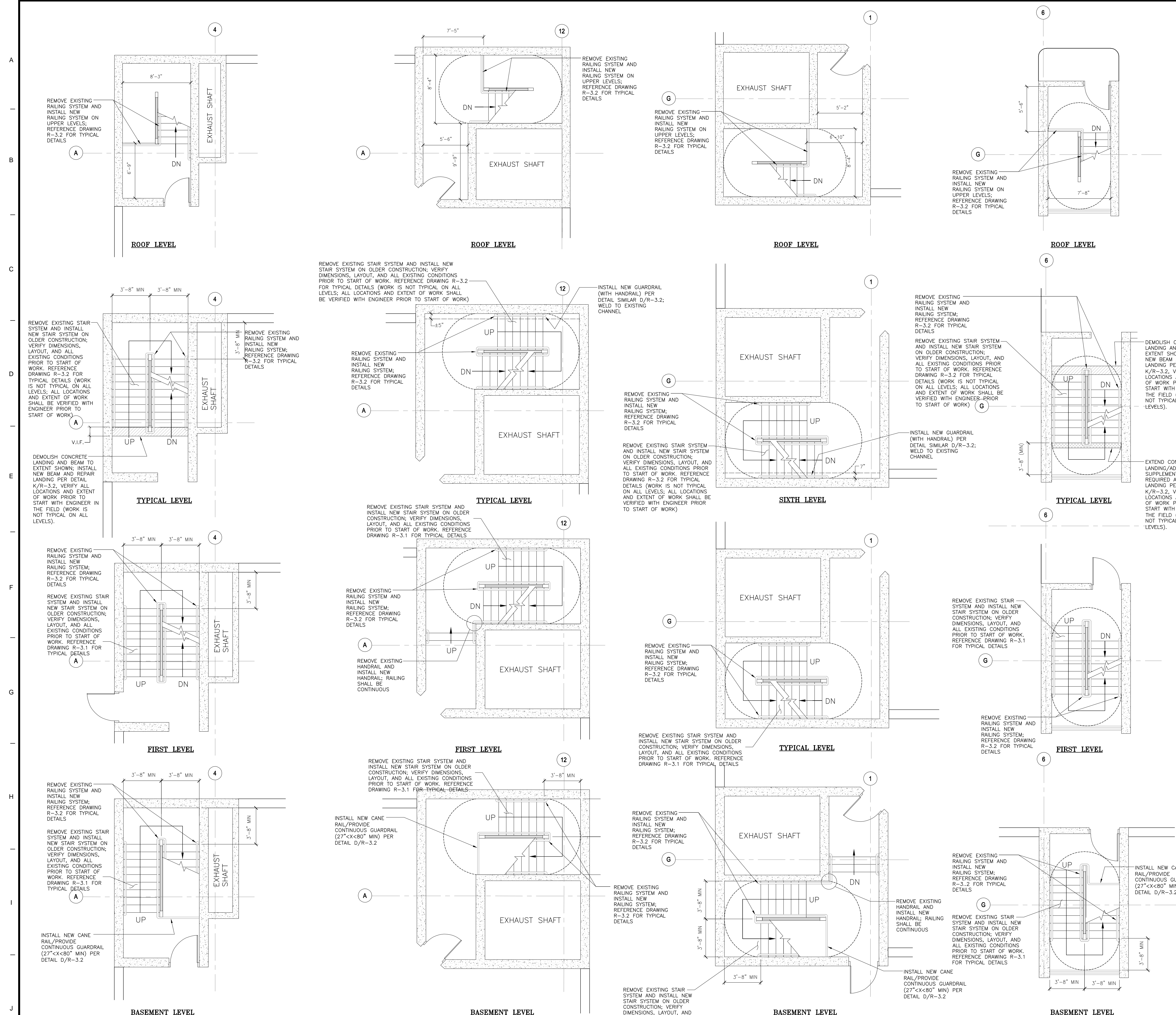


D PARTIAL-DEPTH CONCRETE REPAIR (LEVELS 6 & 7)
R-2.1 SCALE: N.T.S.



E LEVELING REPAIR AT APRON DIFFERENTIALS
R-2.1 SCALE: N.T.S.

Drawing Title CONCRETE REPAIR SECTIONS AND DETAILS		STATE OF CONNECTICUT CAPITAL REGION DEVELOPMENT AUTHORITY	
Revisions		Date: APRIL 2021 Scale: AS NOTED Project Manager: DESMAN Design Management Project: CHURCH STREET PARKING GARAGE RENOVATIONS & IMPROVEMENTS Location: HARTFORD, CONNECTICUT Drawing Number: R-2.1	
No.	Date	Description	Prepared By:
CAD Number:		Project Number:	21-016



- NOTES:**
- CONTRACTOR SHALL NOTE THAT THE CHURCH STREET PARKING GARAGE CONSISTS OF AN ORIGINAL/OLDER PORTION BUILT IN 1954 (THE LOWER LEVELS CONSISTING OF THE BASEMENT UP THROUGH THE APPROXIMATE FIFTH LEVEL) AND A NEWER PORTION ADDED IN 1983 (THE UPPER LEVELS CONSISTING OF THE APPROXIMATE FIFTH LEVEL UP TO THE ROOF). WORK SHALL INCLUDE REPLACEMENT OF THE VARIOUS STAIR ELEMENTS INCLUDING, BUT NOT LIMITED TO, DESIGNATED TREAD/RISEER COMBINATIONS & SUPPORT ANGLES, STRINGER AND OTHER MISCELLANEOUS COMPONENTS, DEPENDING ON DIMENSIONAL CONDITIONS AND REQUIREMENTS, AS FOLLOWS:
 - REGARDING STAIR #1: ALL STAIR COMPONENTS THROUGHOUT THE OLDER PORTION; CONTRACTOR SHALL NOTE THAT WORK SHALL INCLUDE MODIFICATIONS TO IMPACTED LANDINGS IN ORDER TO ACHIEVE MINIMUM DIMENSIONAL REQUIREMENTS, AS DIRECTED BY THE OWNER/ENGINEER, AND THE EXTENT OF MODIFICATIONS SHALL BE VERIFIED WITH THE OWNER/ENGINEER PRIOR TO START OF WORK.
 - REGARDING STAIR #2: ALL STAIR COMPONENTS THROUGHOUT THE OLDER PORTION; CONTRACTOR SHALL REVIEW THE WORK WITH THE OWNER/ENGINEER PRIOR TO START OF WORK.
 - REGARDING STAIR #3: ALL STAIR COMPONENTS THROUGHOUT THE OLDER PORTION; CONTRACTOR SHALL REVIEW THE WORK WITH THE OWNER/ENGINEER PRIOR TO START OF WORK.
 - REGARDING STAIR #4: ALL STAIR COMPONENTS THROUGHOUT BOTH THE OLDER AND NEWER PORTIONS; CONTRACTOR SHALL NOTE THAT WORK SHALL INCLUDE MODIFICATIONS TO IMPACTED LANDINGS IN ORDER TO ACHIEVE MINIMUM DIMENSIONAL REQUIREMENTS, AS DIRECTED BY THE OWNER/ENGINEER, AND THE EXTENT OF MODIFICATIONS SHALL BE VERIFIED WITH THE OWNER/ENGINEER PRIOR TO START OF WORK.
 - ALTHOUGH EXTENT OF REPLACEMENT OF TREADS/RISERS & STRINGERS VARY PER STAIR TOWER SYSTEM, WORK SHALL INCLUDE REPLACEMENT OF THE HANDRAIL & GUARDRAIL SYSTEMS IN THEIR ENTIRETY ON ALL LEVELS IN THE FOUR STAIRS. WORK SHALL INCLUDE REMOVAL OF THE EXISTING RAIL SYSTEMS, PREPARATION OF THE COMPONENTS & SUBSTRATES TO RECEIVE THE NEW RAILING SYSTEMS, FOLLOWED BY THE INSTALLATION OF THE NEW HANDRAIL/GUARDRAIL SYSTEMS. CONTRACTOR SHALL NOTE THAT ALL ANCHOR HARDWARE SHALL BE REPLACED WITH NEW HARDWARE AND ALL WELDING SHALL BE GROUND SMOOTH.
 - CONTRACTOR SHALL NOTE THAT SECTIONS AND DETAILS SHOWN ARE GRAPHICAL IN NATURE, AND SHALL NOT BE INTERPRETED TO BE REPRESENTATIVE OF ALL CONDITIONS. CONTRACTOR SHALL REVIEW EXISTING CONDITIONS PRIOR TO START OF WORK, AND CONTRACTOR SHALL NOTE THAT WORK SHALL INCLUDE THE SUBMISSION OF SHOP DRAWINGS FOR FABRICATION, INSTALLATION AND ERECTION OF METAL SYSTEMS AND COMPONENTS PRIOR TO START OF WORK; INCLUDE PLANS, ELEVATIONS, AND DETAILS OF SECTIONS AND CONNECTIONS, SHOW ANCHORAGE AND ACCESSORY ITEMS, PROVIDE TEMPLATES FOR ANCHOR BOLT INSTALLATION, FABRICATION SHALL NOT PROCEED UNTIL SHOP DRAWINGS HAVE BEEN REVIEWED. FABRICATION, ASSEMBLY, INSTALLATION AND ERECTION SHALL CONFORM TO REVIEWED SHOP DRAWINGS; SHOP DRAWINGS SHALL BE STAMPED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF CONNECTICUT.
 - ALL NEW COMPONENTS SHALL BE HOT-DIPPED GALVANIZED; TOUCH UP ALL FIELD WELDING TO HOT DIP GALVANIZED ELEMENTS AS RECOMMENDED BY COATING MANUFACTURER AND APPROVED BY ENGINEER.
 - WORK SHALL INCLUDE REPAIR OF DELAMINATED/SPALLED CONCRETE THROUGHOUT THE STAIRS (I.E. LANDINGS AND TREADS); CONTRACTOR SHALL LOCATE AREAS FOR REPAIR PRIOR TO START OF WORK FOR SUBSEQUENT VERIFICATION BY THE ENGINEER. CONTRACTOR SHALL NOTE THAT REPAIR TO THE CONCRETE AND METAL COMPONENTS DO NOT NECESSARILY COINCIDE WITH EACH OTHER. CONTRACTOR SHALL REVIEW THE STAIRS IN THEIR ENTIRETY WITH THE ENGINEER PRIOR TO THE START OF WORK.
 - FOLLOWING COMPLETION OF ALL METAL AND CONCRETE WORK, WORK SHALL INCLUDE THE PAINTING OF THE STAIR SYSTEM IN ITS ENTIRETY. WORK SHALL CONSIST OF SURFACE PREPARATION BY APPROVED METHOD, PRIMING AND PAINTING OF THE STAIR SYSTEMS; FINAL COLOR SHALL BE CHOSEN BY OWNER, AND MAY CONSIST OF MULTIPLE COLORS AS DETERMINED BY THE OWNER.
 - CONTRACTOR SHALL NOTE THAT LEAD PAINT CURRENTLY EXISTS THROUGHOUT THE STAIRS; THEREFORE, CONTRACTOR SHALL COMPLY WITH ALL APPLICABLE REQUIREMENTS FOR PROPER CONTAINMENT, REMOVAL AND DISPOSAL.
 - WORK SHALL ALSO INCLUDE MISCELLANEOUS REPAIR TO THOSE COMPONENTS REMAINING AND EXCLUDED FROM REPLACEMENT, TYPICALLY ASSOCIATED WITH THE NEWER PORTION OF THE GARAGE. UPON REVIEW AND DIRECTION BY THE OWNER/ENGINEER, CONTRACTOR SHALL PROVIDE APPLICABLE PRICING FOR REVIEW; FOLLOWING REVIEW OF THE CONTRACTOR'S PRICING, OWNER/ENGINEER SHALL AUTHORIZE THE REPAIR OF DESIGNATED ITEMS. CONTRACTOR SHALL NOT PROCEED WITH THE ADDITIONAL MISCELLANEOUS REPAIRS UNTIL WRITTEN AUTHORIZATION IS RECEIVED.
 - STAIR REPAIR/REPLACEMENT SHALL INCLUDE INSTALLATION OF PLYWOOD PARTITIONS WITH LOCKED ACCESS, BAGGING OF EXISTING EXIT SIGNS AND TEMPORARY EXIT SIGNAGE DIRECTING PEDESTRIANS TO ADJACENT STAIRWELLS AT EACH LEVEL PRIOR TO THE START OF WORK.

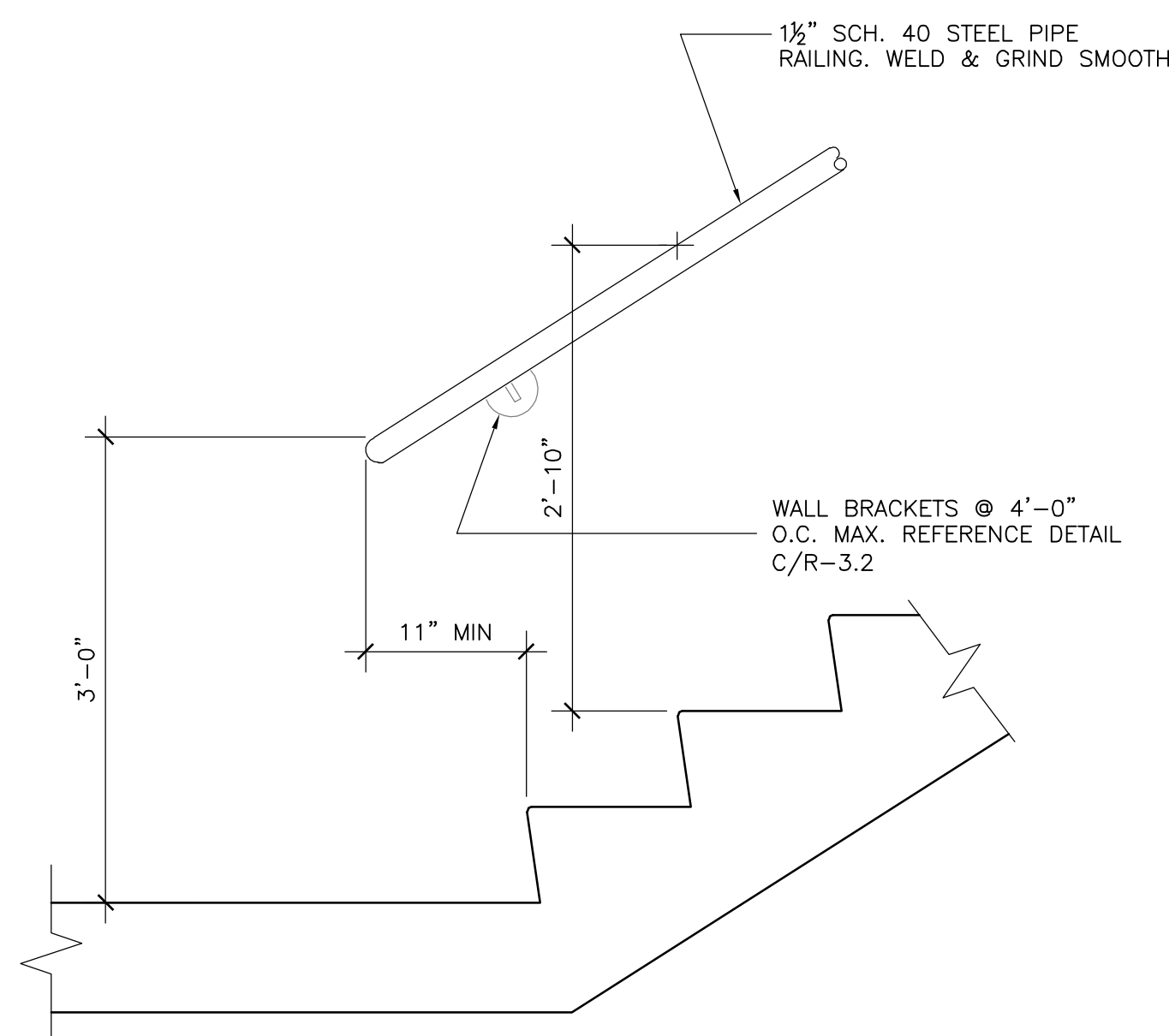
PARTIAL PLAN - STAIR #1
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PARTIAL PLAN - STAIR #2
SCALE: 1/4"=1'-0"

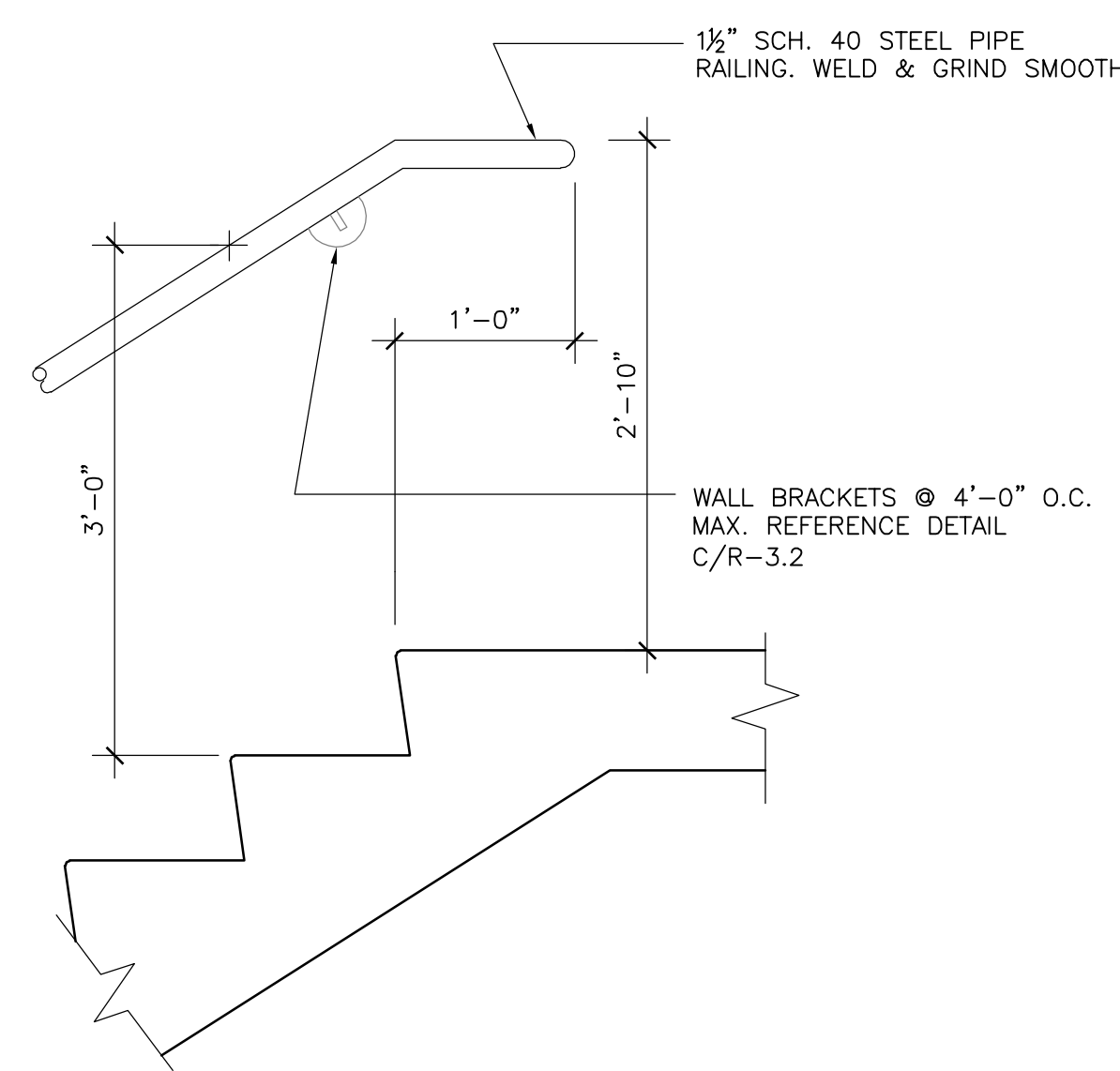
PARTIAL PLAN - STAIR #3
SCALE: 1/4"=1'-0"

PARTIAL PLAN - STAIR #4
SCALE: 1/4"=1'-0"

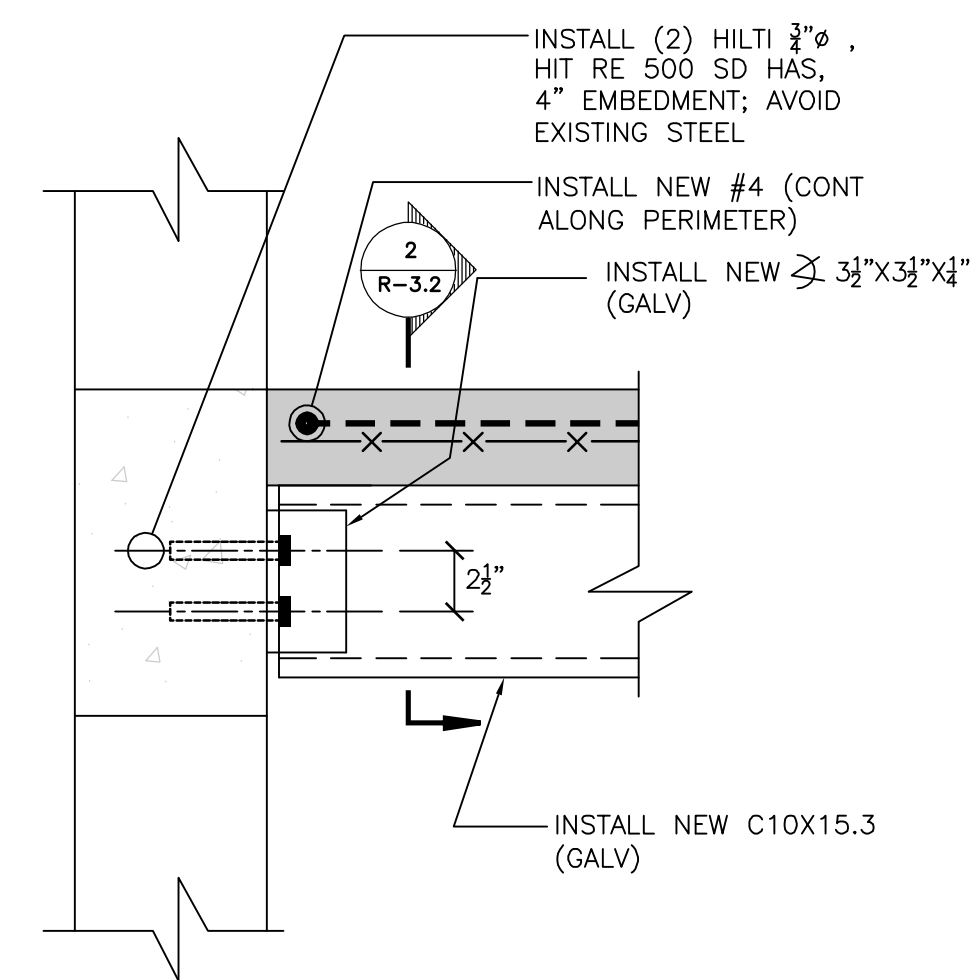
Drawing Title		STATE OF CONNECTICUT	
MISCELLANEOUS METAL SECTIONS & DETAILS		CAPITAL REGION DEVELOPMENT AUTHORITY	
Revisions	Date	Description	Drawn By
No.	Date	Description	Drawn By
Project		CHURCH STREET PARKING GARAGE RENOVATIONS & IMPROVEMENTS	
Project Address		HARTFORD, CONNECTICUT	
Project Manager		DESIGN MANAGEMENT	
Production Leader		AS NOTED	
Project Number		21-016	
Drawing Number		R-3.1	



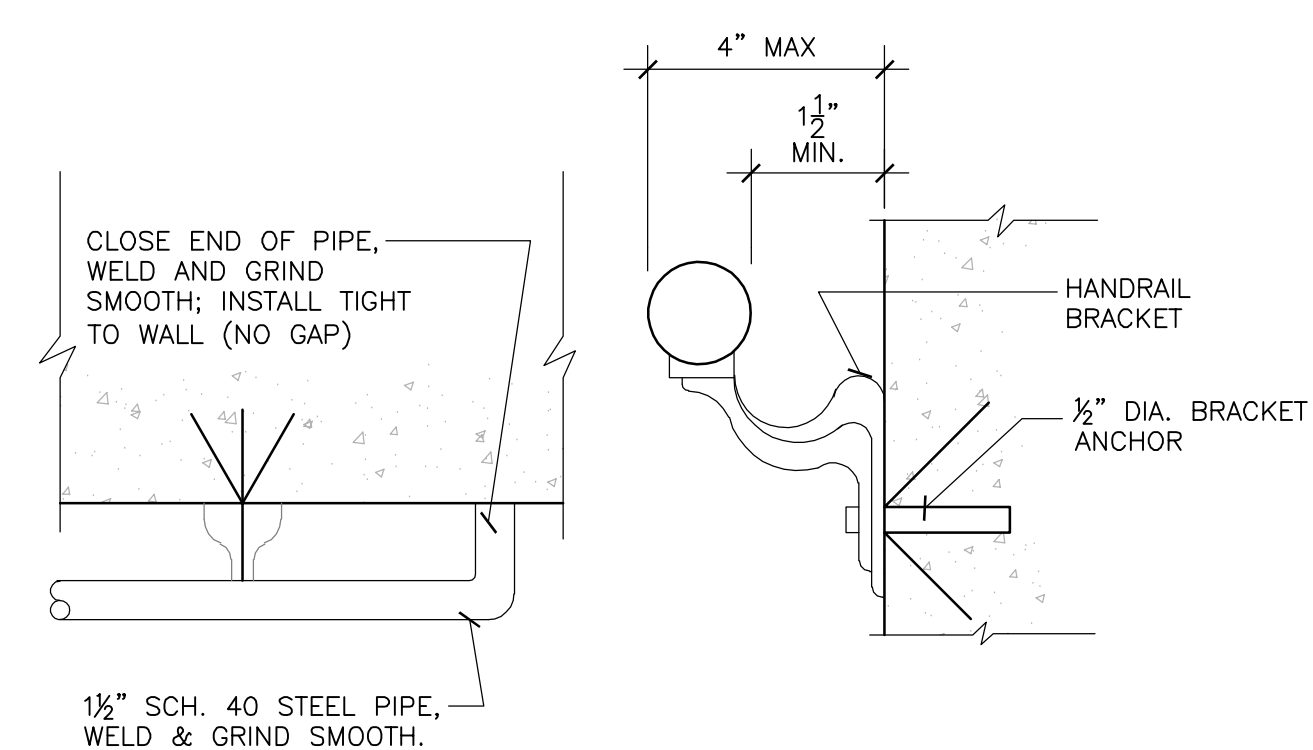
A LOWER HANDRAIL MOUNTING
R-3.2 SCALE: N.T.S.



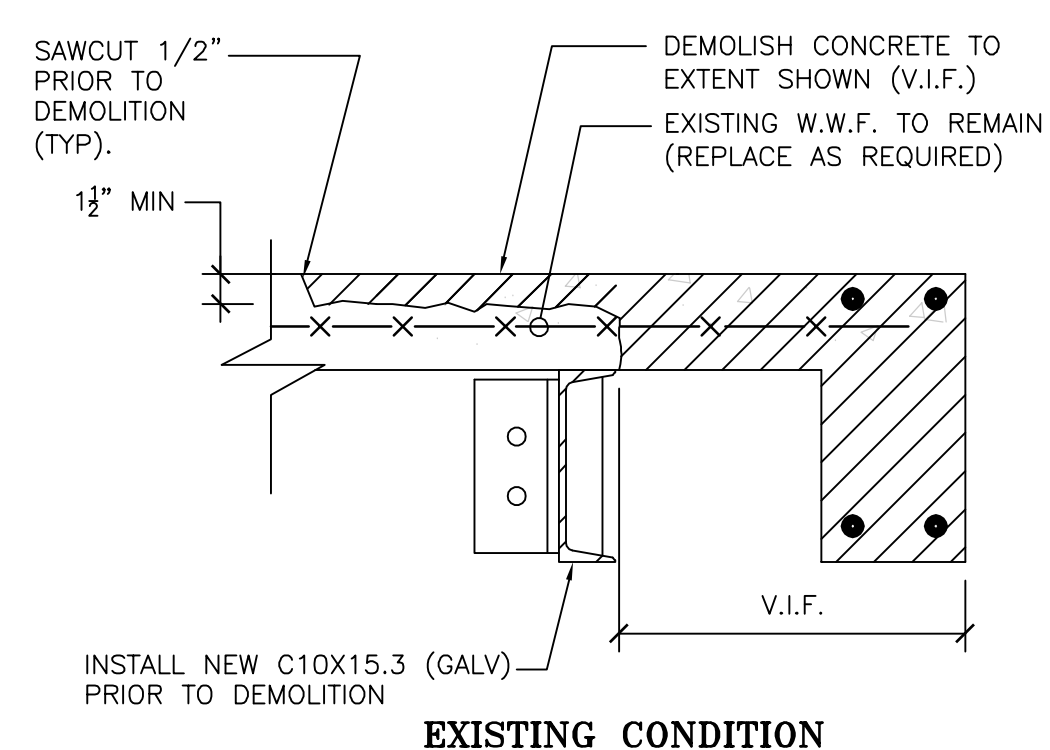
B UPPER HANDRAIL MOUNTING
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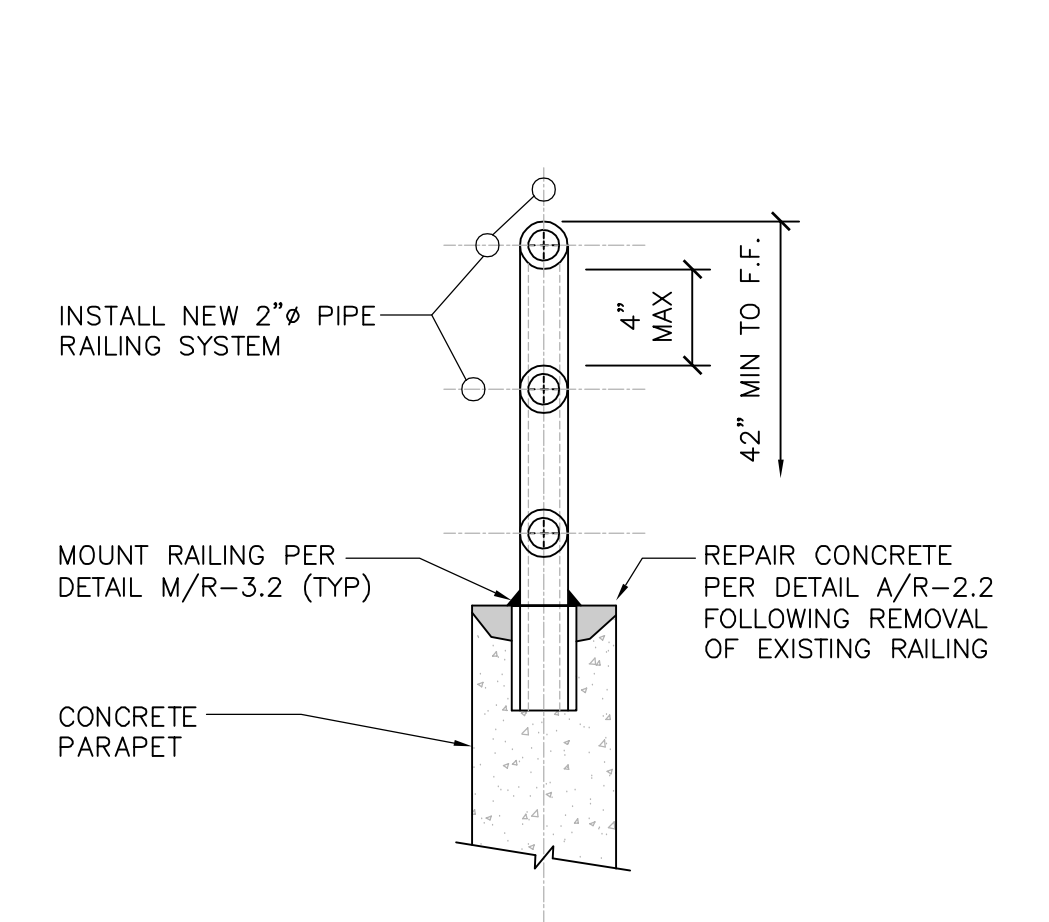
K INSTALLATION OF NEW BEAM AT UPPER LANDING
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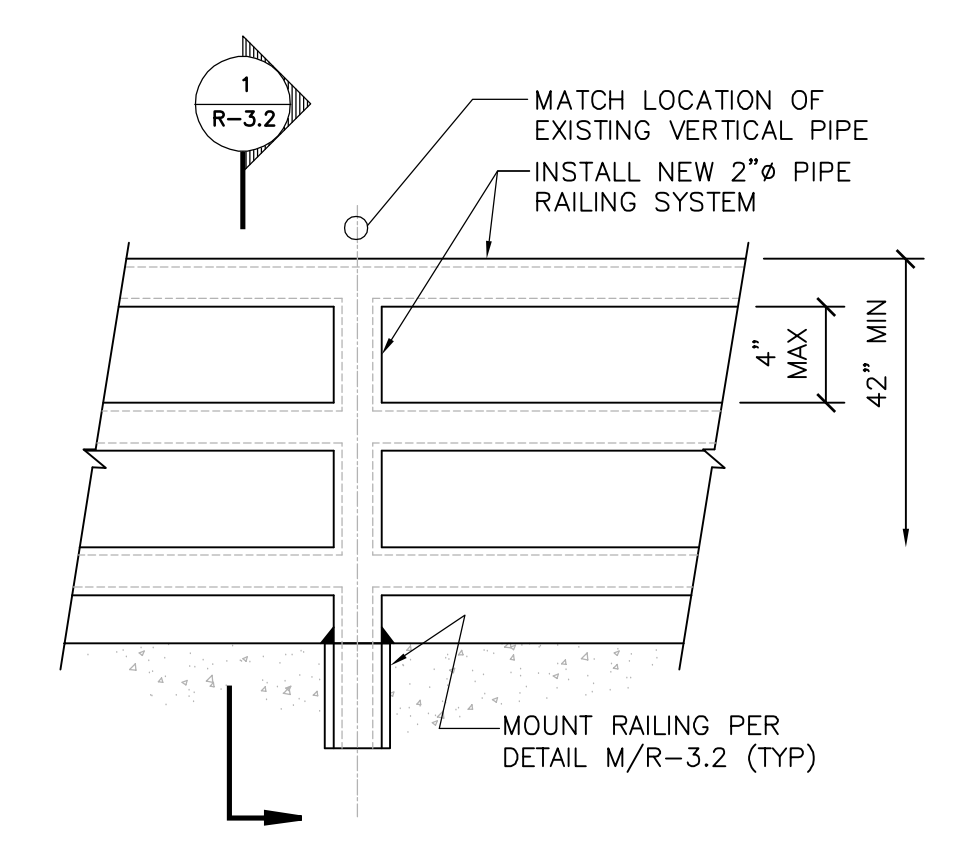
C HANDRAIL INSTALLATION @ WALL
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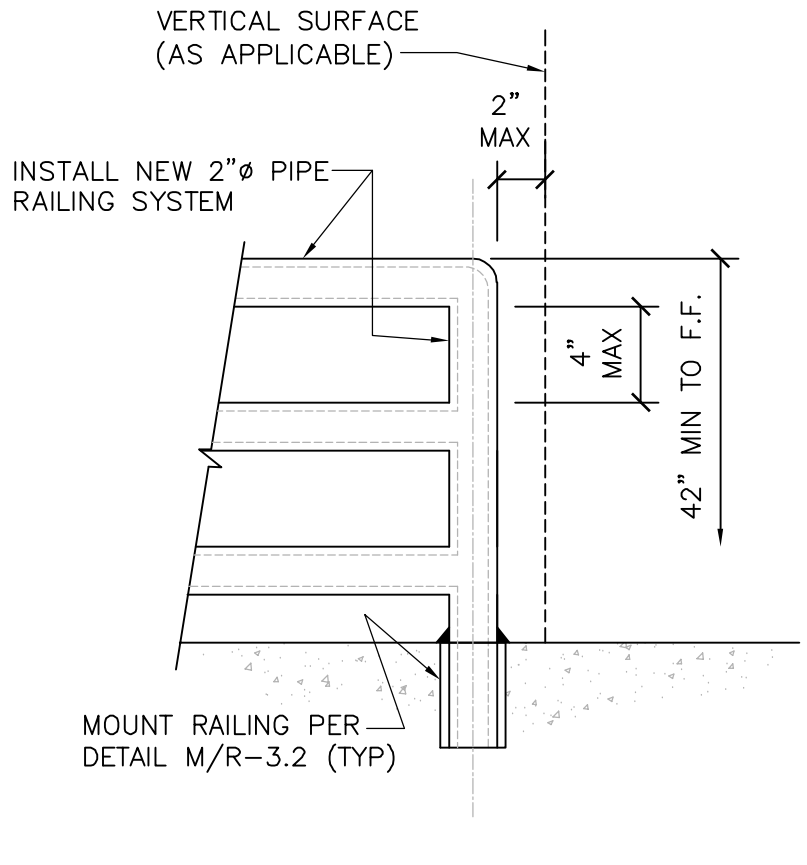
2 SECTION
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1 SECTION
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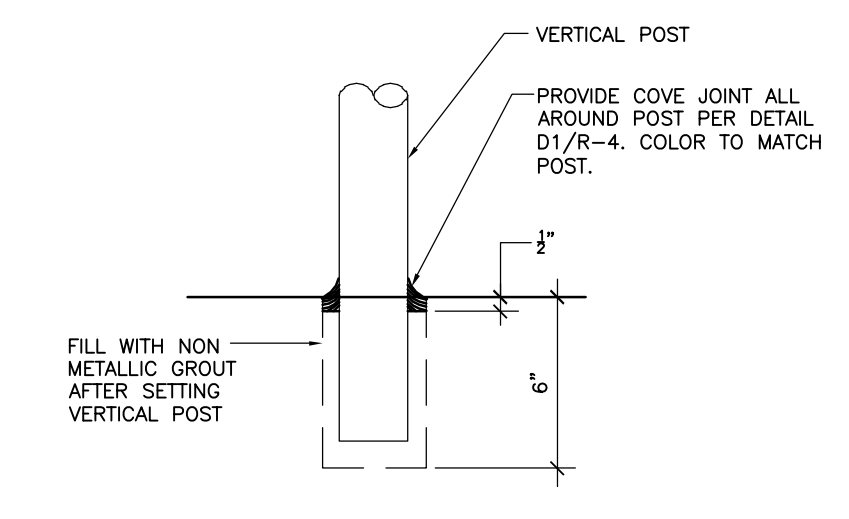


L1 EXTERIOR SPANDREL

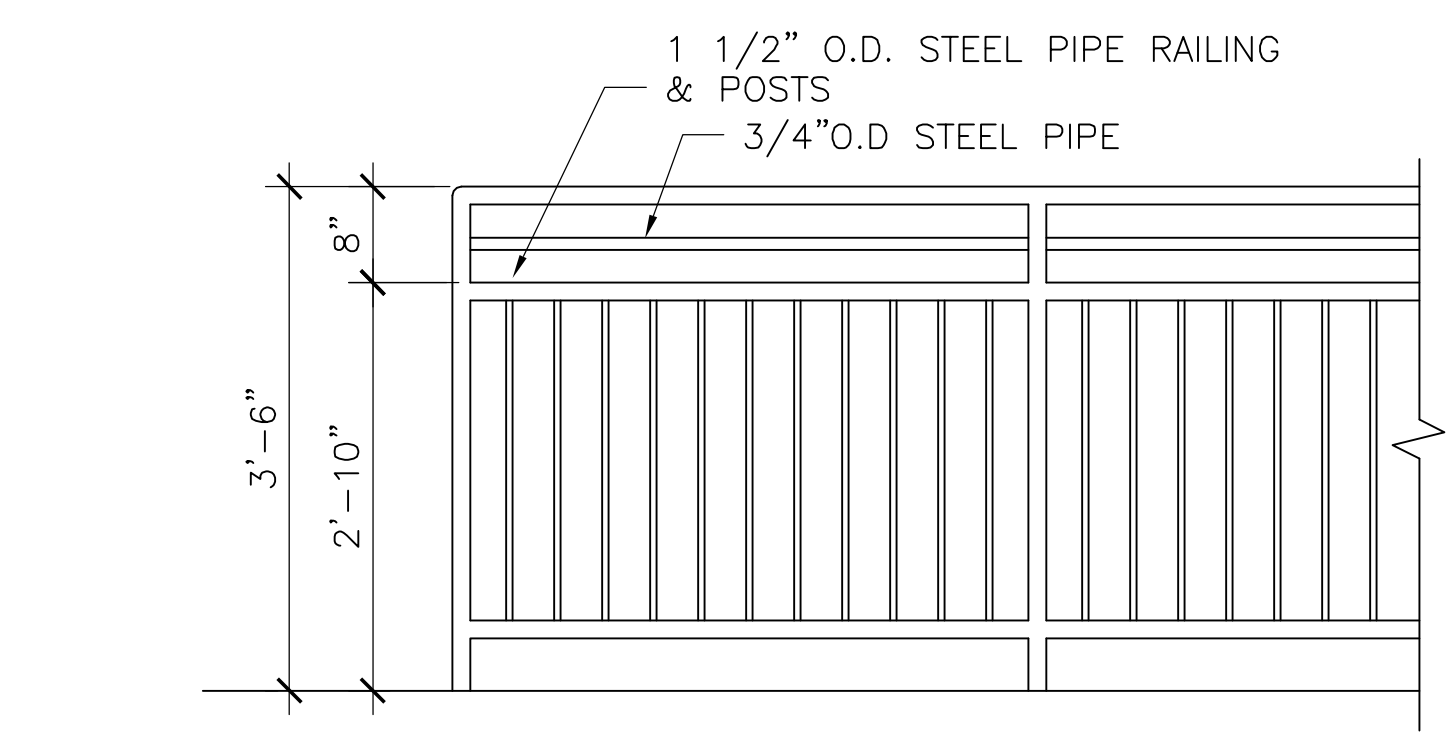


L2 INTERIOR SPANDREL

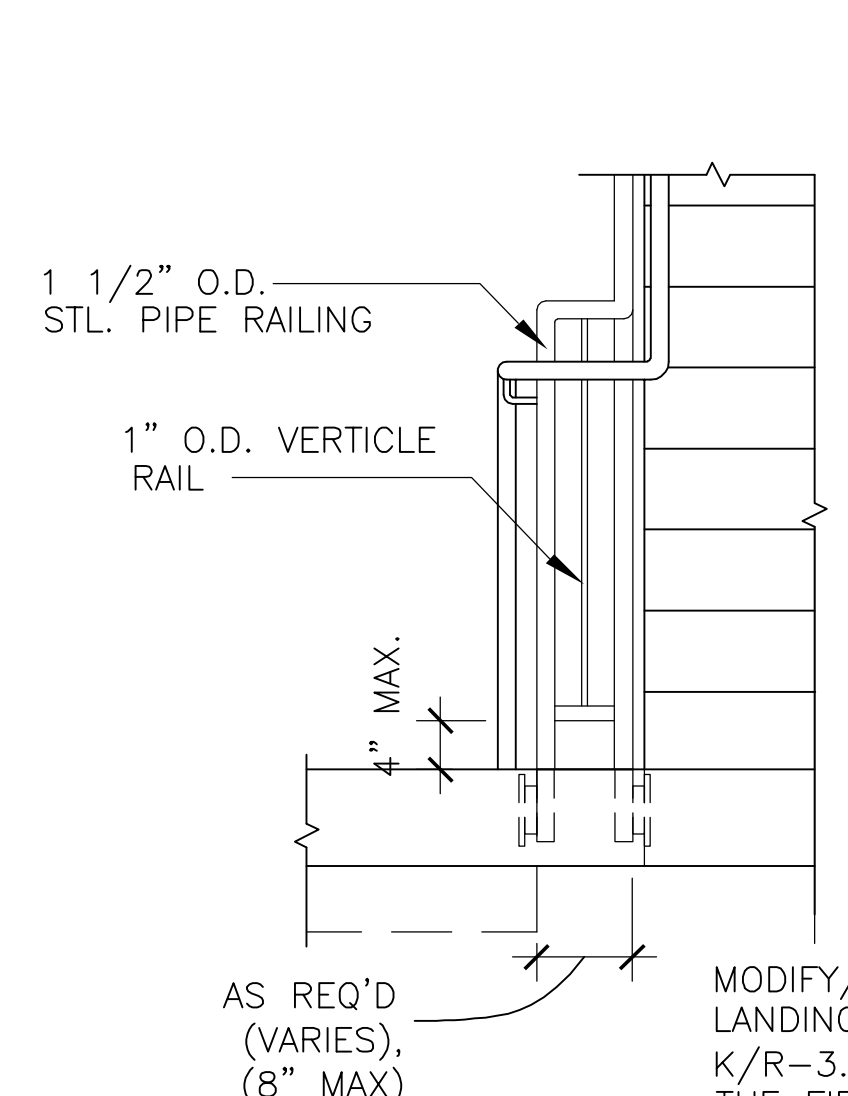
L PARAPET RAILING REPLACEMENT
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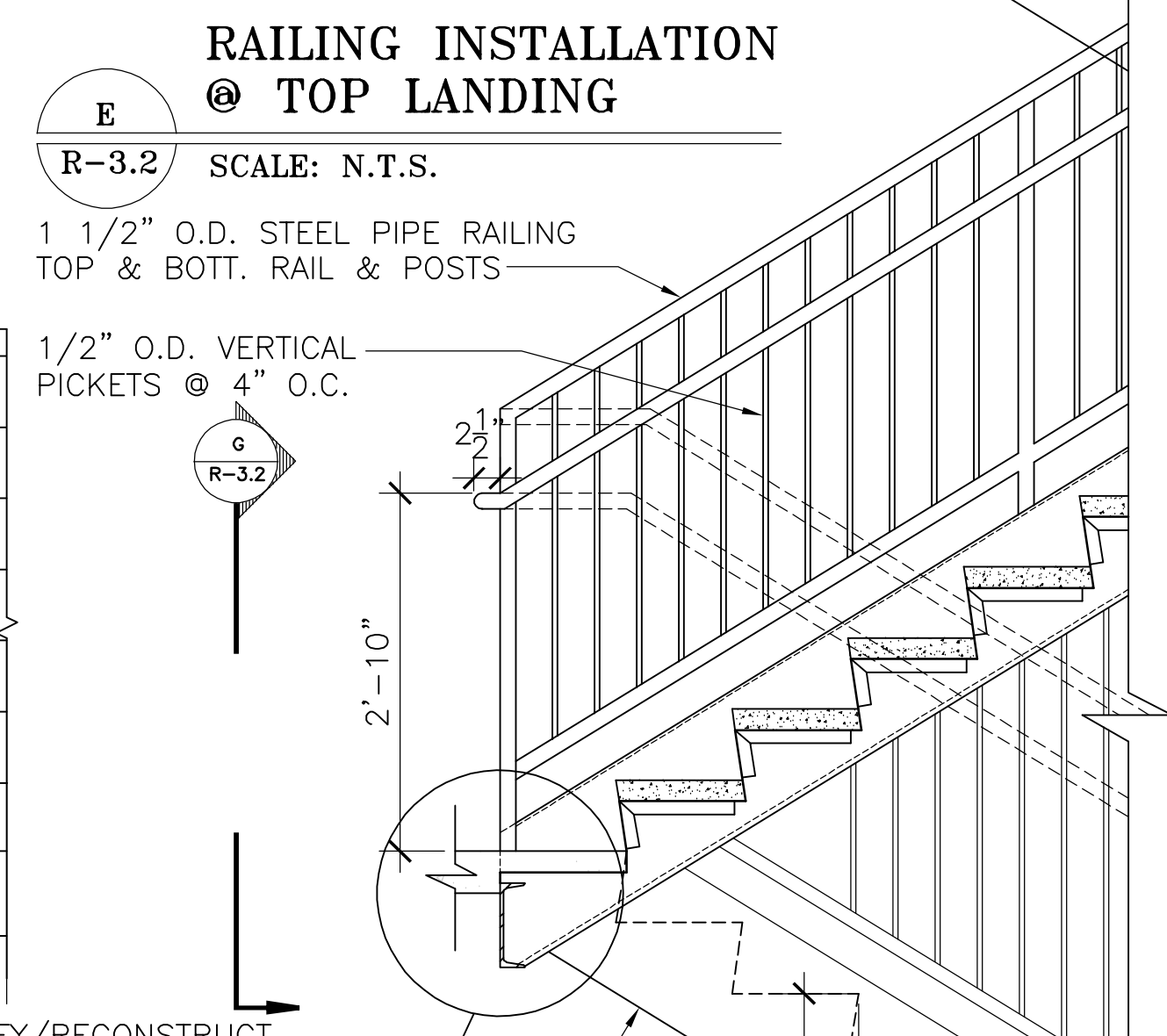
M POST INSTALLATION
R-3.2 SCALE: N.T.S.



D GUARDRAIL INSTALLATION
R-3.2 SCALE: N.T.S.



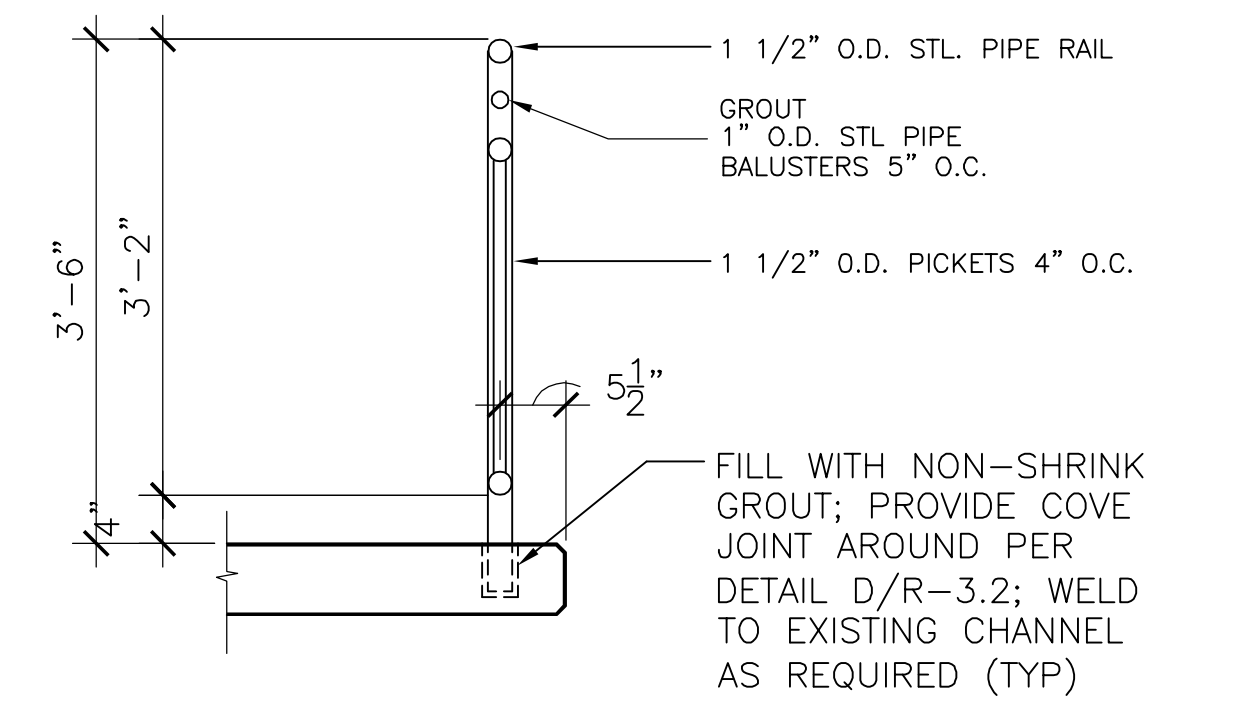
G CROSS-SECTION THROUGH FACE
R-3.2 SCALE: N.T.S.



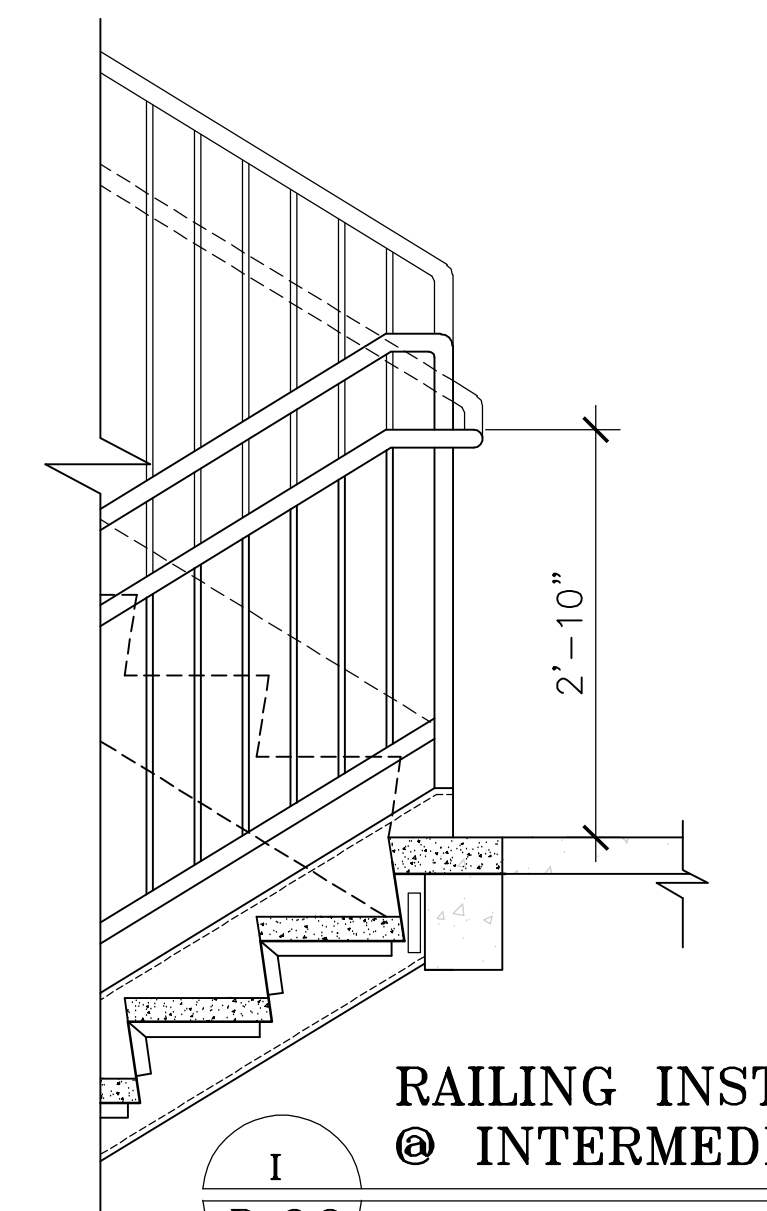
E RAILING INSTALLATION @ TOP LANDING
R-3.2 SCALE: N.T.S.

H RAILING INSTALLATION @ MAIN LANDING
R-3.2 SCALE: N.T.S.

J RAILING INSTALLATION @ GRADE LANDING
R-3.2 SCALE: N.T.S.

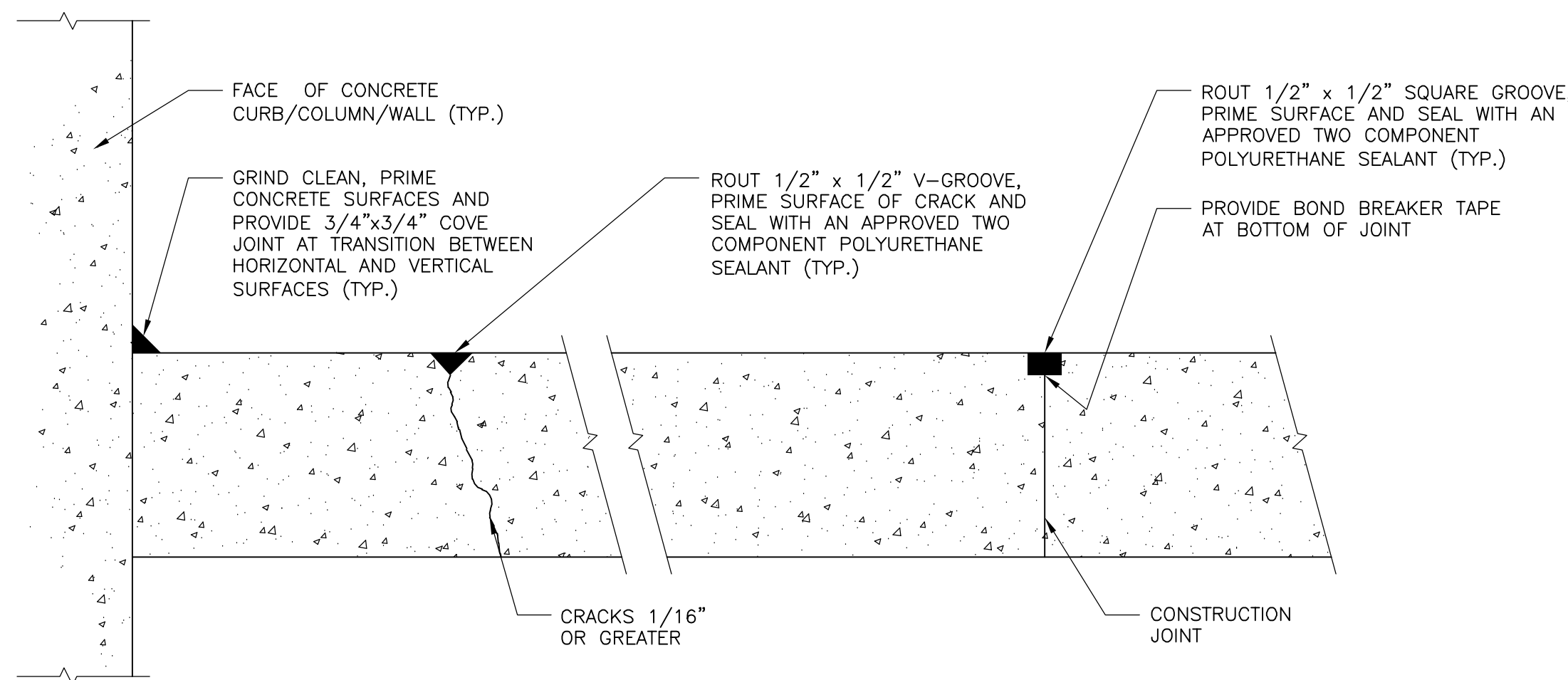


F SECTION THROUGH GUARDRAIL INSTALLATION
R-3.2 SCALE: N.T.S.



I RAILING INSTALLATION @ INTERMEDIATE LANDING
R-3.2 SCALE: N.T.S.

Drawing Title		STATE OF CONNECTICUT CAPITAL REGION DEVELOPMENT AUTHORITY	
MISCELLANEOUS METAL SECTIONS & DETAILS		DESMAN Design Management Project: CHURCH STREET PARKING GARAGE RENOVATIONS & IMPROVEMENTS HARTFORD, CONNECTICUT Project Number: 21-016	
Revisions		Date: APRIL 2021 Scale: AS NOTED Project Manager: - Project Architect: - Peer Reviewer: - Drawing Number: R-3.2	
No.	Date	Description	Planned By:
CAD Number:		Project Number: 21-016	



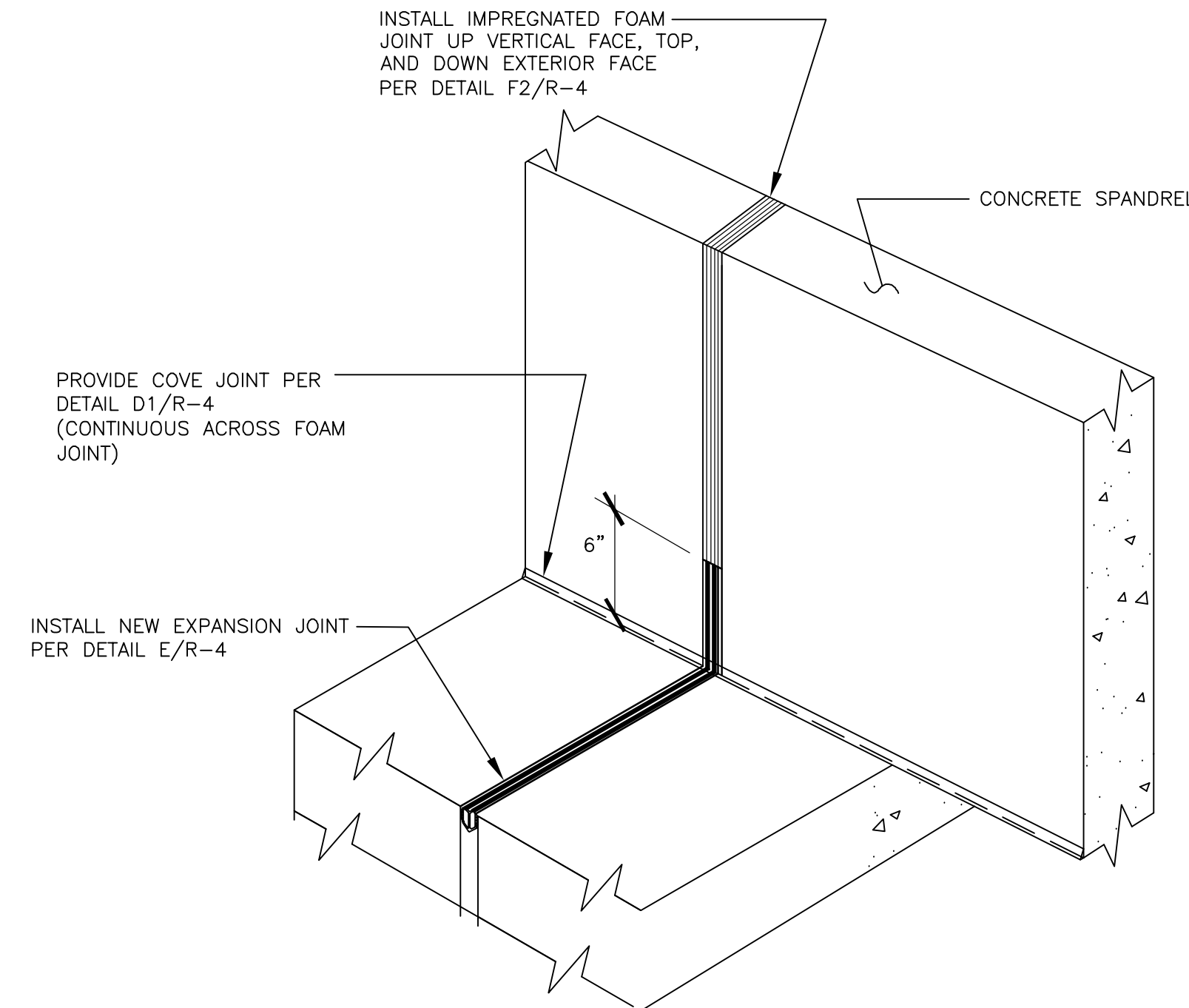
DETAIL D1 **DETAIL D2** **DETAIL D3**

REPAIR PROCEDURE:

1. CONCRETE SURFACES SHALL BE FULLY DRY AND CURED PRIOR TO INSTALLATION OF APPROVED SEALANT MATERIAL.
2. ROUT/GRIND CRACKS AND JOINTS AS REQUIRED, CLEAN PROFILED SURFACES USING COMPRESSED AIR OR OTHER MEANS TO REMOVE DUST, DEBRIS AND OTHER MATERIAL, WHICH MIGHT IMPAIR THE SEALANT BOND TO THE SUBSTRATE.
3. PRIME SURFACES TO RECEIVE SEALANT, PROVIDE BOND BREAKER TAPE (AS REQUIRED) AND SEAL WITH AN APPROVED TWO COMPONENT POLYURETHANE SEALANT AS SHOWN.
4. WHERE SEALANT IS TO BE LEFT EXPOSED TO FORM A CONCAVE PROFILE, WHERE THE SEALANT WILL BE COVERED BY A WATERPROOFING MEMBRANE LEAVE FLUSH WITH FLOOR SURFACE.
5. PROHIBIT VEHICULAR AND PEDESTRIAN TRAFFIC ON SEALED JOINTS UNTIL SEALANT MATERIAL HAS CURED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS.

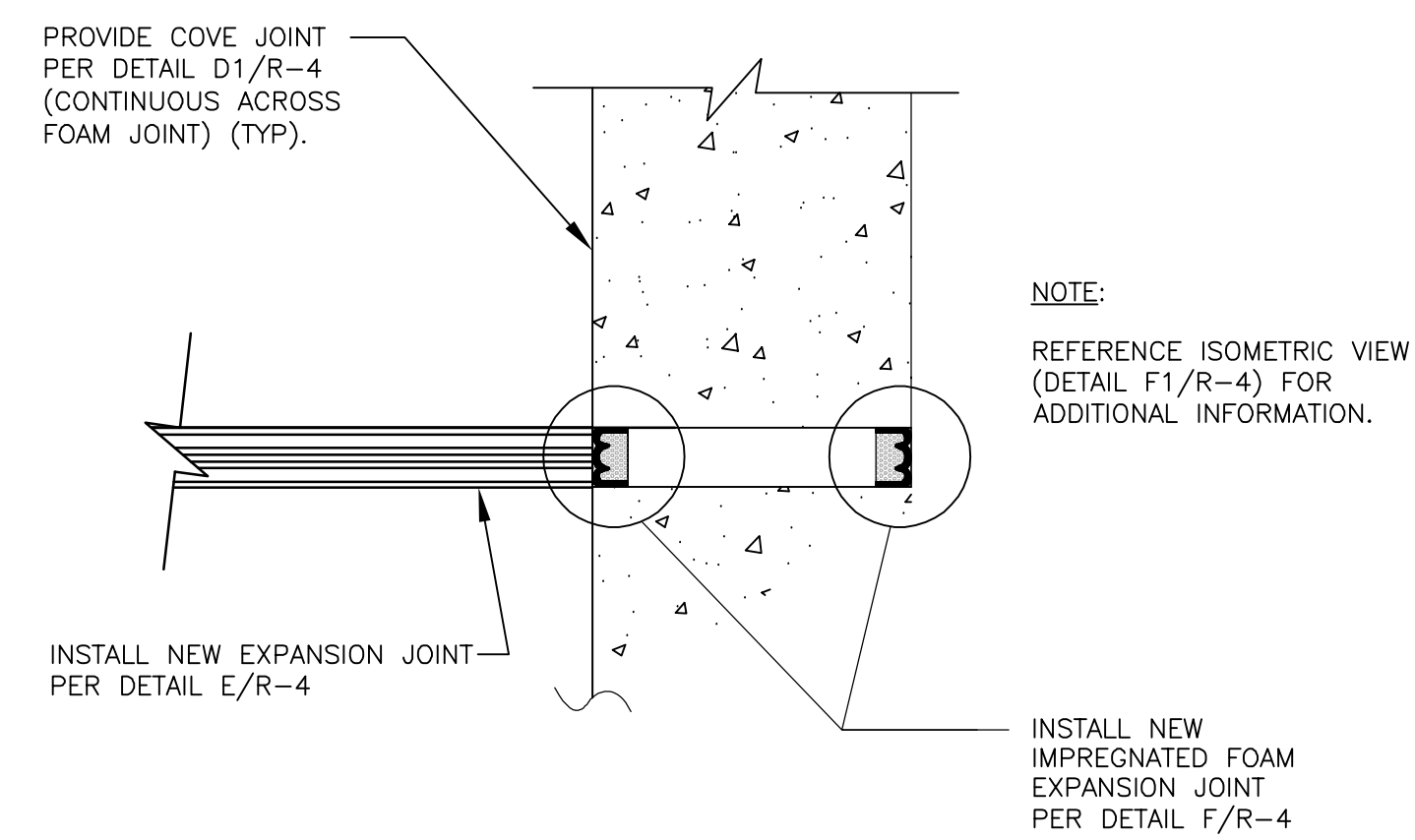
D COVE JOINT/CRACK/CONSTRUCTION JOINT

R-4 SCALE: N.T.S.



F1 ISOMETRIC VIEW - EXPANSION JOINT AT PARAPET

R-4 SCALE: N.T.S.



F2 SECTION - EXPANSION JOINT AT PARAPET

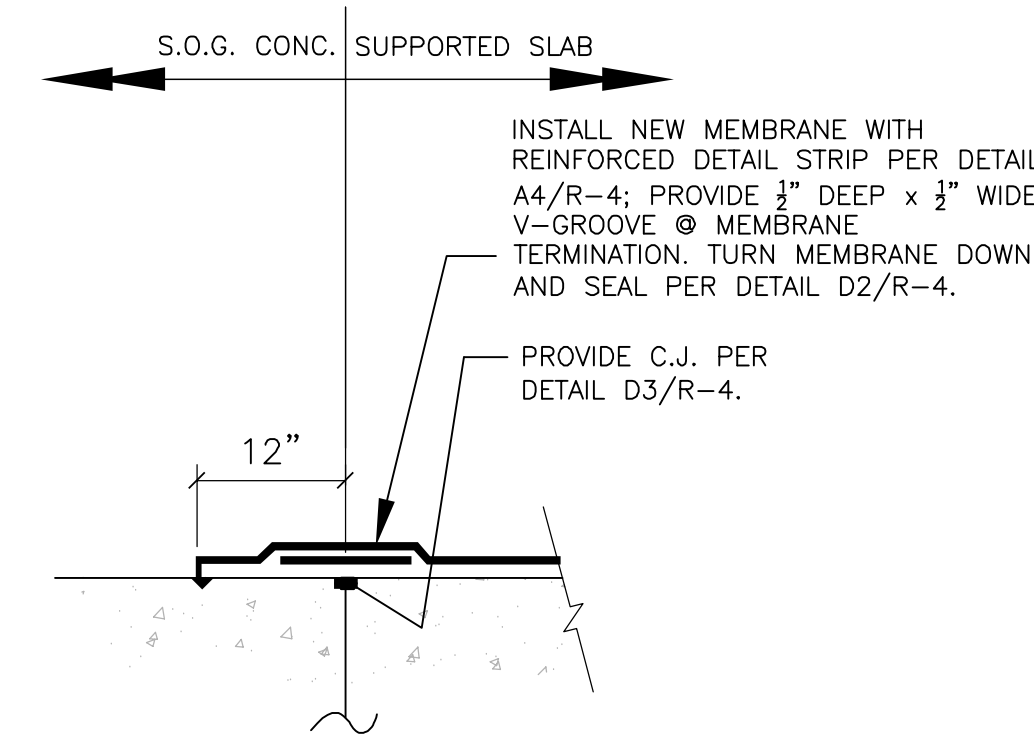
R-4 SCALE: N.T.S.

EXPANSION JOINT INSTALLATION NOTES:

1. PREPARE CONCRETE BLOCKOUT TO THE DIMENSION SHOWN OR AS MIGHT OTHERWISE BE REQUIRED BY EXPANSION JOINT MANUFACTURER AND SUBSEQUENTLY APPROVED BY THE ENGINEER.
2. SAWCUT, GRIND, OR OTHERWISE PREPARE CONCRETE BLOCKOUT, PERFORMING ALL NECESSARY CONCRETE REPAIRS REQUIRED, TO MEET THE JOINT MANUFACTURER'S INSTALLATION REQUIREMENTS.
3. REMOVE ALL DEBRIS AND FOREIGN MATERIAL FROM THE CONCRETE BLOCKOUT WHICH MIGHT PREVENT PROPER INSTALLATION.
4. CONTRACTOR SHALL FIELD VERIFY ALL JOINT WIDTHS AND INSTALL THE APPROPRIATE SIZED EXPANSION JOINT GLAND AS PER MANUFACTURER'S RECOMMENDATIONS. ALL EXPANSION JOINT INSTALLATION DETAILS (I.E. BENDS, TURNS, TERMINATIONS, ETC.) SHALL BE PER MANUFACTURER'S STANDARD DETAILS AS APPROVED BY THE ENGINEER.
5. INSTALL SPECIFIED EXPANSION JOINT GLAND AS PER MANUFACTURER'S RECOMMENDATIONS. A CERTIFIED MANUFACTURER'S REPRESENTATIVE SHALL APPROVE OF CONCRETE BLOCKOUT PREPARATION, INSTALLATION PROCEDURES AND FINAL INSTALLATION IN WRITING.
6. CONTRACTOR SHALL NOTE THAT THE JOINT ELEMENTS SHALL BE A CONTINUOUS, FACTORY EXTRUDED UNIT FOR THE ENTIRE STRAIGHT RUN LENGTH OF THE JOINT, CONTINUING THROUGH CURBING AS REQUIRED.
7. THE SEAL ELEMENT SHALL NOT BE MITERED/JOINED. CHANGES IN DIRECTION OR ELEVATION SHALL BE ACCOMPLISHED BY FACTORY SPLICING AND FACTORY MOLDED ELBOWS, TEES, CROSSES AND THE LIKE, AND SHALL NOT BE PERFORMED IN THE FIELD, UNLESS APPROVED BY THE ENGINEER. THE SEAL SHALL BE TURNED UP A MINIMUM OF 6 INCHES (VERTICALLY) UNLESS OTHERWISE SHOWN ON PLANS. ALL TERMINATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS AND APPROVED BY THE ENGINEER SO AS TO PROVIDE A WATERTIGHT SEAL.

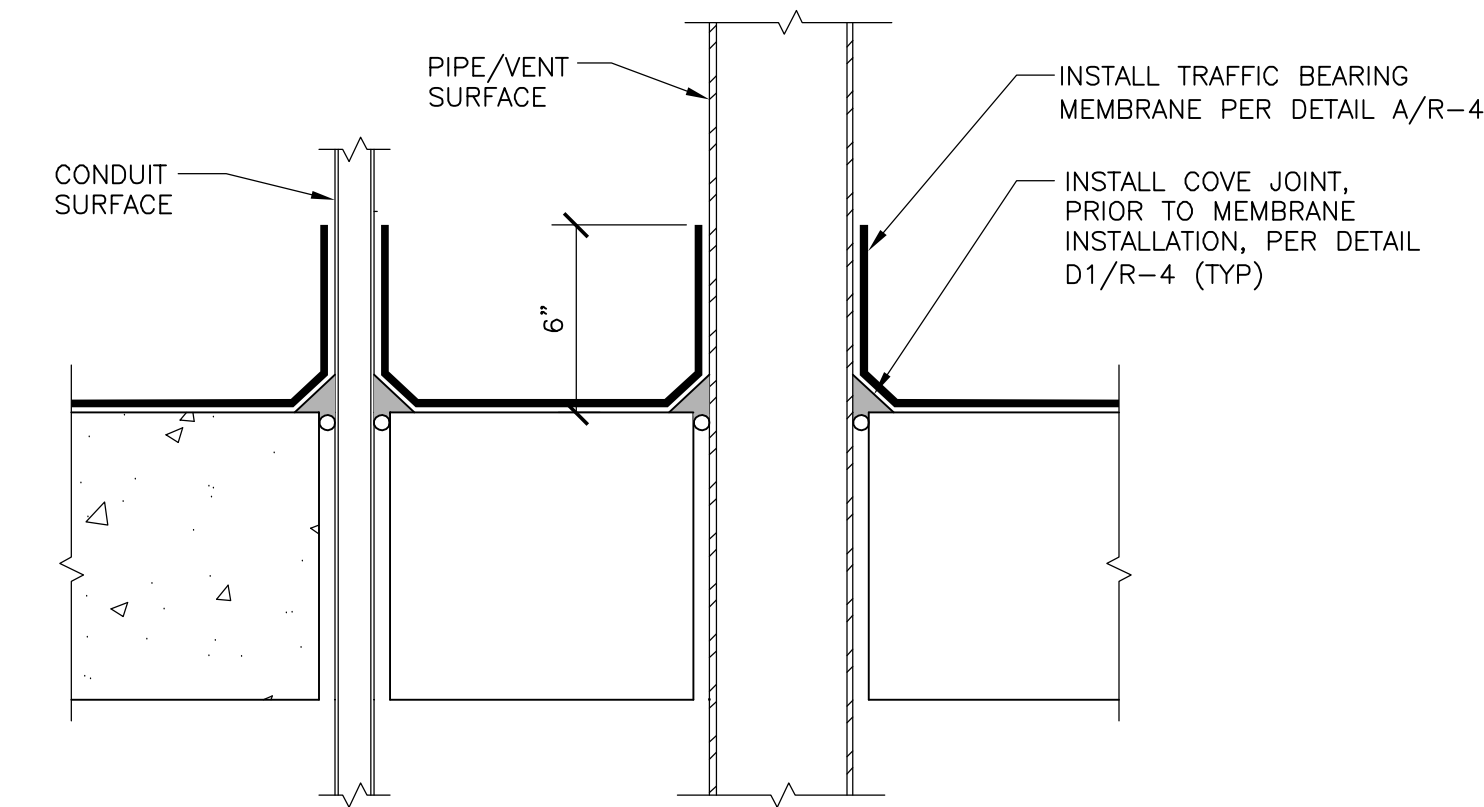
REPAIR PROCEDURE:

1. SHOTBLAST CLEAN HORIZONTAL SURFACES AND SANDBLAST CLEAN VERTICAL SURFACES SCHEDULED TO RECEIVE TRAFFIC BEARING WATERPROOFING MEMBRANE. OIL STAINS, PAINT AND OTHER CONTAMINANTS SHALL BE REMOVED AS RECOMMENDED BY THE SYSTEM MANUFACTURER.
2. SURFACE PREPARATION SHALL ALSO INCLUDE REMOVING ANY EXISTING DETERIORATED MEMBRANE, GRINDING HIGH RIDGES IN THE EXISTING CONCRETE AND NEWLY CONSTRUCTED AREAS, WHICH MAY BE DETRIMENTAL TO THE SYSTEM PERFORMANCE. SYSTEM MANUFACTURER WILL BE EXPECTED TO ACCEPT FINAL DECK PREPARATION PRIOR TO MEMBRANE APPLICATION.
ALL CRACKS GREATER THAN 1/16" AND CONTROL/CONSTRUCTION JOINTS SHALL BE ROUTED AND SEALED WITH AN APPROVED TWO-PART POLYURETHANE PER DETAILS D2 & D3/R-4. PLACE A 3/4" BY 3/4" COVE JOINT AT ALL INTERFACES BETWEEN HORIZONTAL AND VERTICAL SURFACES AS PER DETAIL D1/R-4.
3. THE SYSTEM MANUFACTURER AND COATING APPLICATOR SHALL PERFORM THE FOLLOWING:
A. VERIFY THE MOISTURE CONTENT OF THE EXISTING AND NEW CONCRETE AREAS.
B. PERFORM ADHESION TESTING ON EXISTING MEMBRANE AND NEW CONCRETE AREAS TO VERIFY COMPLIANCE WITH THE SYSTEM PERFORMANCE. DECK PREPARATION SHALL BE PER SPECIFICATION IN ALL TEST AREAS.
C. THE SYSTEM MANUFACTURER AND COATING APPLICATOR SHALL SUBMIT A JOINT LETTER STATING THAT THE ABOVE TESTING WAS PERFORMED, THAT THE SURFACE PREPARATION IS ACCEPTABLE AND THAT THE SYSTEM WILL PERFORM PER SPECIFICATIONS, AS APPLIED ON THE CONCRETE SLAB.
4. PROVIDE APPROPRIATE DETAILINGS, AS RECOMMENDED BY MANUFACTURER AND APPROVED BY ENGINEER.
5. APPLY THE APPROPRIATE (CONCRETE, METAL, ETC.) PRIMER PER MANUFACTURER'S RECOMMENDATIONS.
6. DETAIL CRACKS WITH BASE COAT(S) AS PER MANUFACTURER'S RECOMMENDED PROCEDURE.
7. APPLY BASE COAT TO SPECIFIED THICKNESS.
8. APPLY THE WEARING COURSE TO THE SPECIFIED THICKNESS AND COLOR. INSTALL MEMBRANE IN LAYERS, IF REQUIRED, AS PER MANUFACTURER'S RECOMMENDATION.
9. INSTALL THE LOCK COAT OR TOP COAT AS PER SYSTEM REQUIREMENTS.



B TERMINATION AT SLAB-ON-GRADE

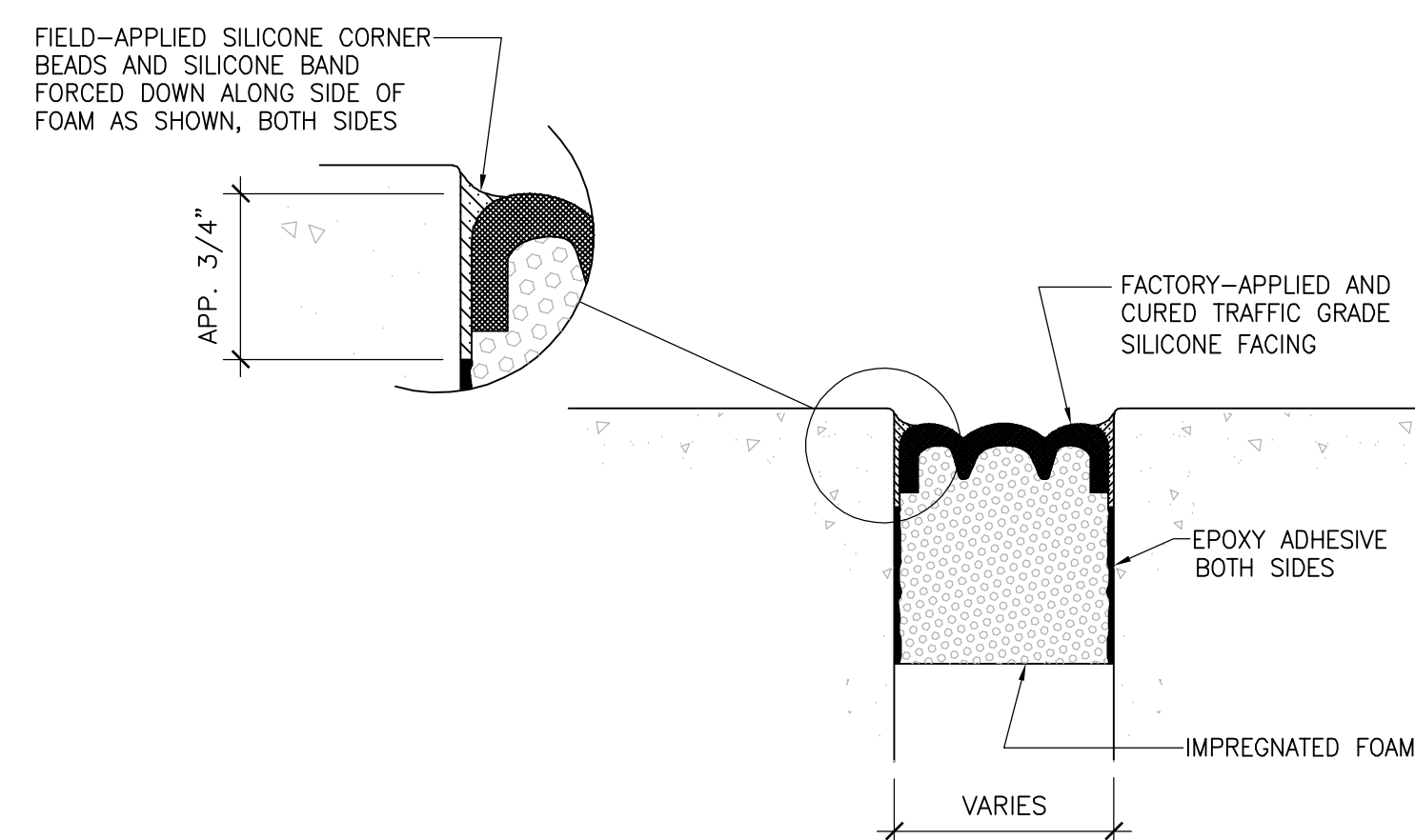
R-4 SCALE: N.T.S.



NOTE:
CONTRACTOR SHALL COORDINATE USE OF APPROPRIATE PRIMER WITH MANUFACTURER, AS APPROVED BY ENGINEER, FOR SUBSTRATE REQUIREMENTS.

C TERMINATION AT VERTICAL SURFACES

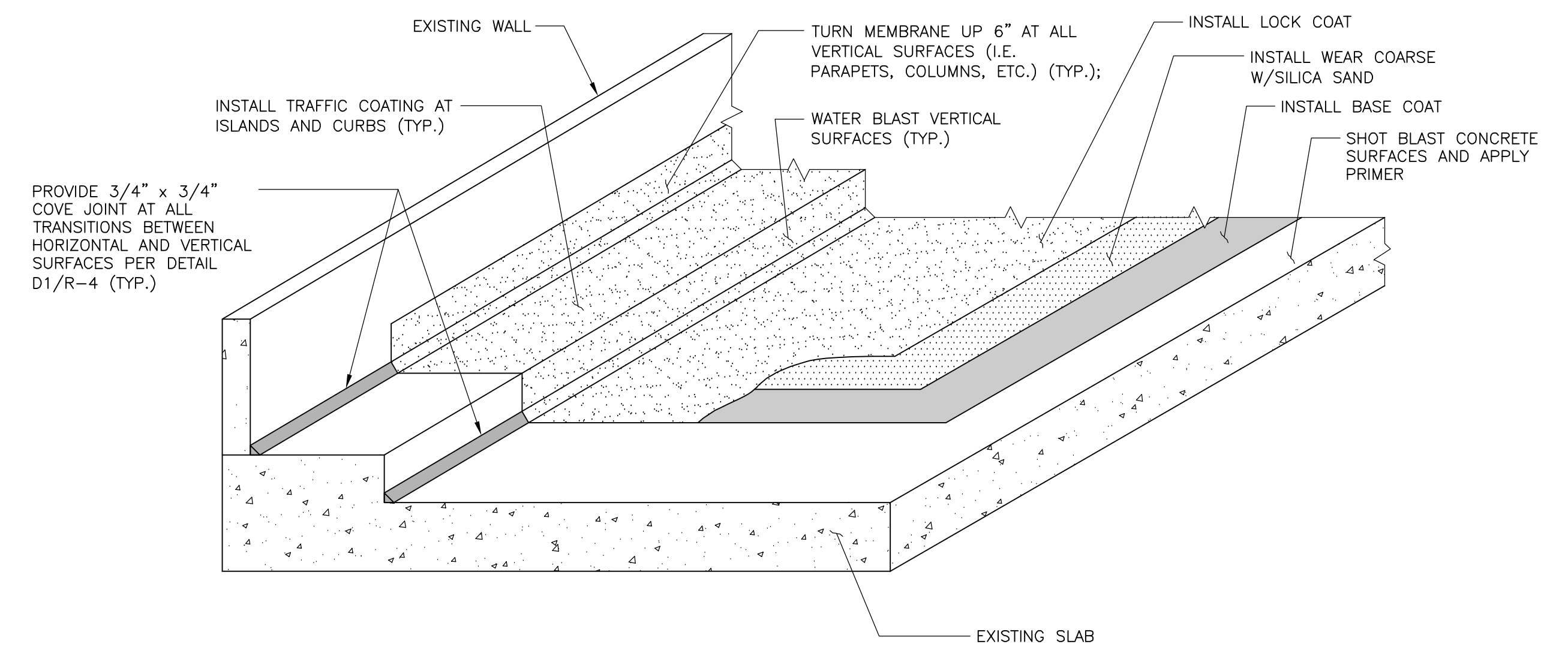
R-4 SCALE: N.T.S.



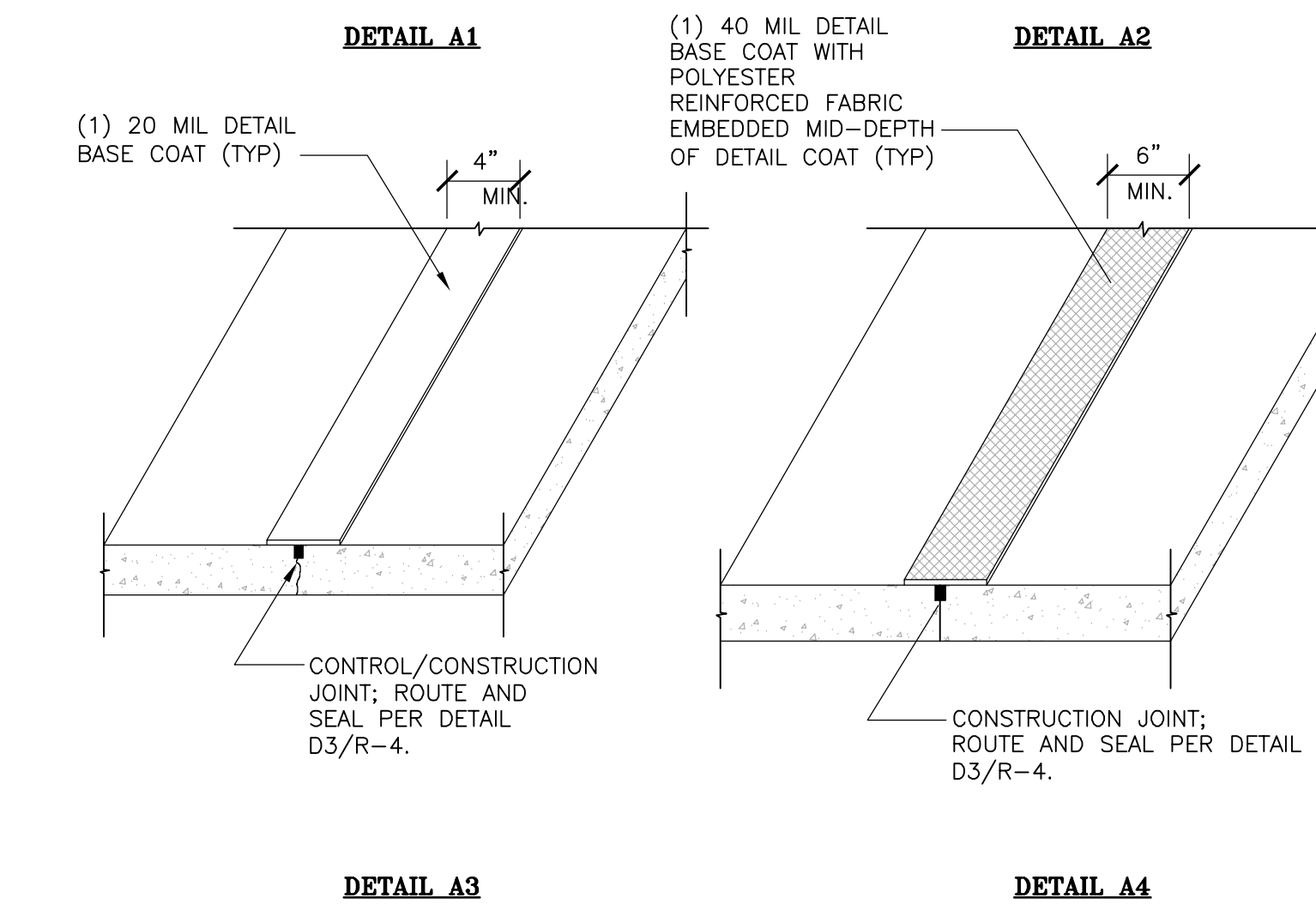
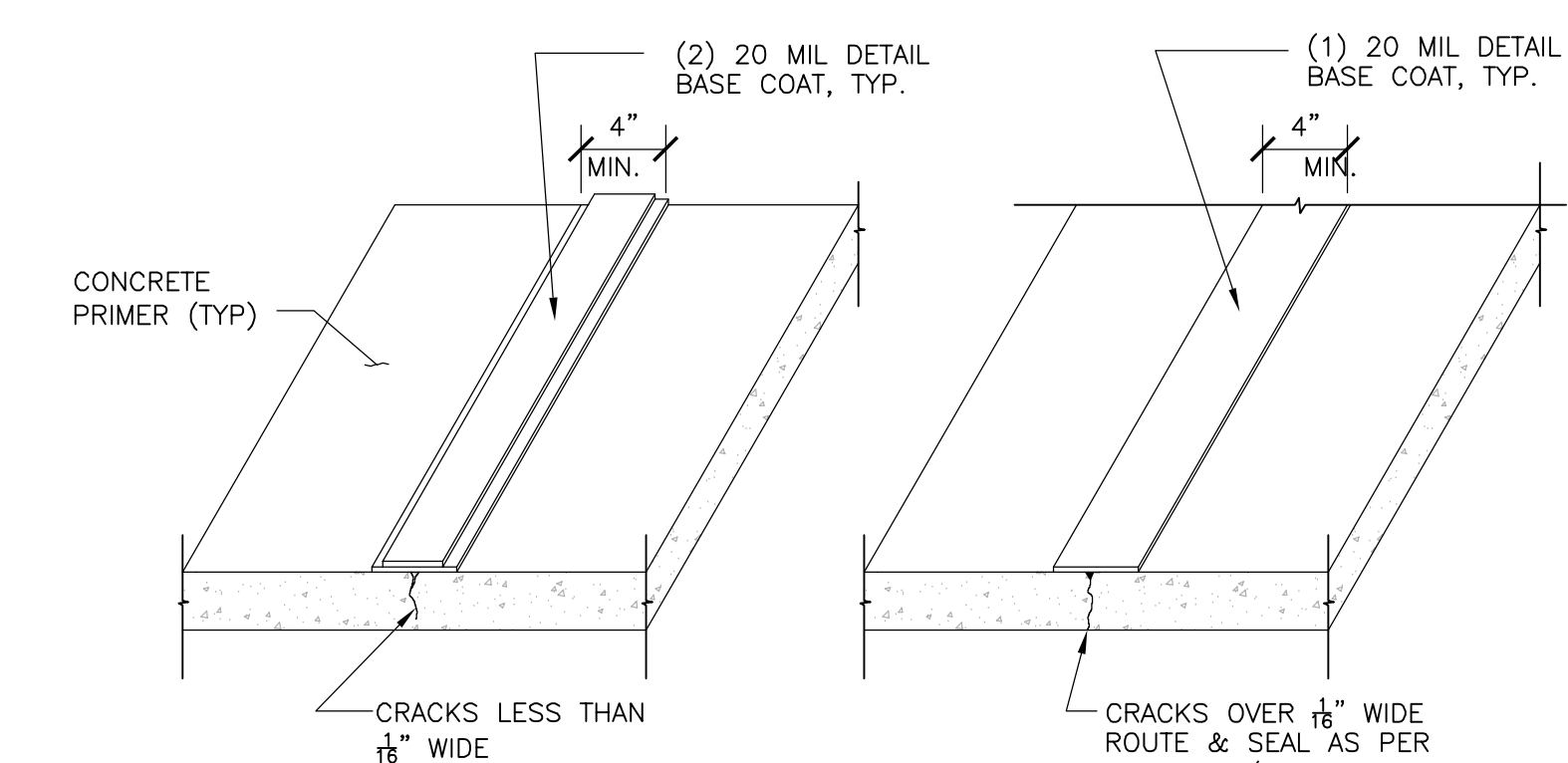
ACCEPTABLE JOINT SYSTEM:
JOINT SYSTEM SHALL BE HSEAL, EH-SERIES, AS MANUFACTURED BY WATSON BOWMAN ACME, CORP., TEL: (716) 691-7566

F INSTALLATION OF NEW PRE-COMPRESSED, IMPREGNATED FOAM EXPANSION JOINT SEAL

R-4 SCALE: N.T.S.

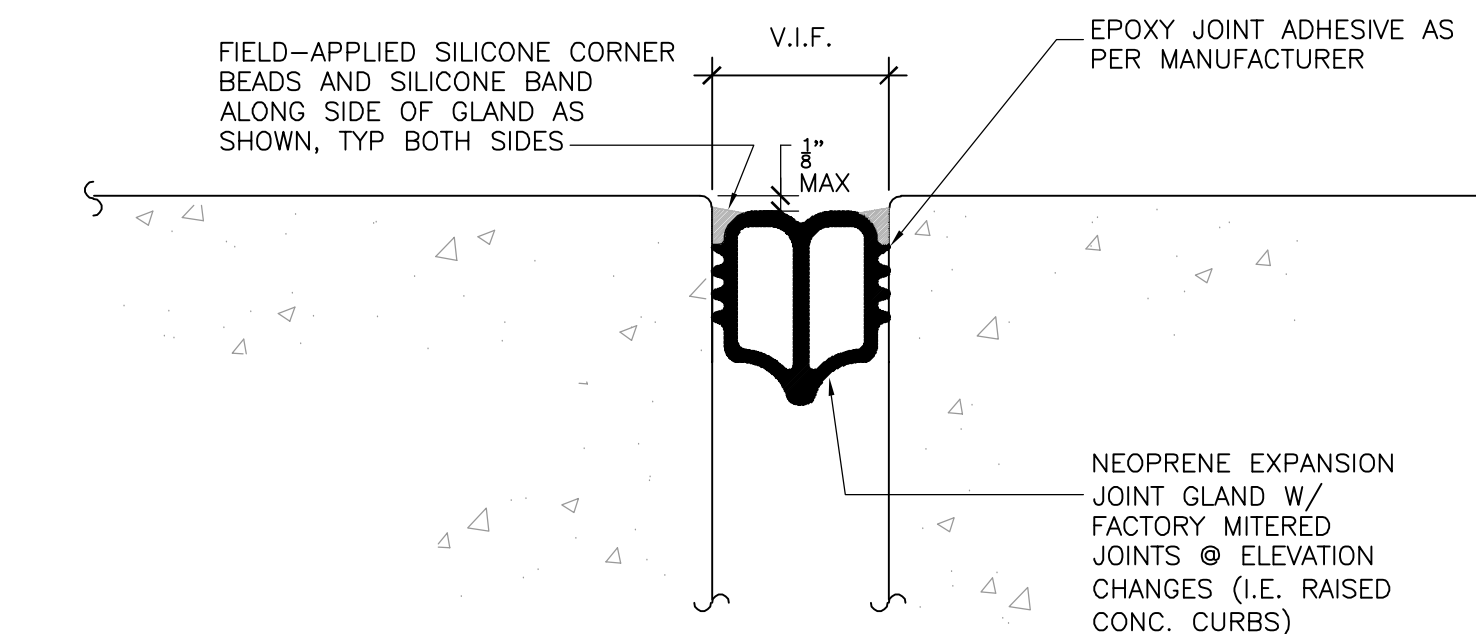


ISOMETRIC



A TRAFFIC BEARING MEMBRANE INSTALLATION

R-4 SCALE: N.T.S.



ACCEPTABLE JOINT SYSTEM:
JOINT SYSTEM SHALL BE JEENE, FW-SERIES, AS MANUFACTURED BY WATSON BOWMAN ACME, CORP., TEL: (716) 691-7566, OR APPROVED EQUAL

E INSTALLATION OF NEW EPOXY-ADHERED, AIR-PRESSURIZED NEOPRENE EXPANSION JOINT SEAL

R-4 SCALE: N.T.S.

Drawing Title		STATE OF CONNECTICUT CAPITAL REGION DEVELOPMENT AUTHORITY	
WATERPROOFING DETAILS AND SECTION		Date: APRIL 2021	
Revisions		Status: AS NOTED	
No.	Date	Description	Project Number: -
-		Project Leader: -	
-		Production Manager: -	
-		Project: CHURCH STREET PARKING GARAGE RENOVATIONS & IMPROVEMENTS	
-		Project Abstract: -	
-		Field Reviewer: -	
-		Drawing Number: R-4	
CAD Number: -		Project Number: 21-016	